

SIGNING

COUNTY OF	ITEM NO.	SHEET NO.
Campbell	6-2021.00	T01

ESTIMATE OF QUANTITIES

NOTES :

- (1) PAYMENT FOR GROUND MOUNTED SIGN SUPPORTS TYPE A AND TYPE C SHALL BE BASED ON THE NOMINAL WEIGHT OF THE BEAMS. THE NECESSARY GALVANIZING, HARDWARE, ETC. IS TO BE CONSIDERED INCIDENTAL. QUANTITIES FOR TYPE C SUPPORTS SHALL INCLUDE ALL NECESSARY HARDWARE TO FORM COMPLETE BREAK-AWAY BEAMS. SEE PANEL SIGN DETAIL SHEET.
- (2) QUANTITY SHALL INCLUDE ALL COPY AND HARDWARE NECESSARY TO FORM COMPLETE SIGNS. NO DEDUCTION IN AREA IS TO BE MADE FOR ROUNDING OF CORNERS.
- (3) WITH PERMISSION OF THE ENGINEER, SHEETING SIGNS ON THE RAMPS AND SIDE ROADS MAY BE MOVED TO BE COMPATIBLE WITH THE EXISTING SIGNS.
- (4) QUANTITY IS ESTIMATED. THE EXACT LENGTH SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
- (5) WHERE REQUIRED, BRACING FOR SHEETING SIGNS SHALL BE INCIDENTAL TO STEEL POST. SEE SHEETING SIGN DETAIL SHEET.
- (6) QUANTITY SHALL INCLUDE ALL MATERIAL NECESSARY TO FORM A COMPLETE BREAK-AWAY ASSEMBLY. TYPE I POSTS AND CONCRETE SHALL BE PAID SEPARATELY. SEE SHEETING SIGN DETAIL SHEET.
- (7) QUANTITY SHALL INCLUDE SIGN AND POST.
- (8) ALL MATERIALS REMOVED AND NOT REUSED, SUCH AS SIGNS, SIGN LIGHTS, SIGN SUPPORTS, ETC. SHALL BECOME THE PROPERTY OF THE CONTRACTOR.
- (9) THE REMOVAL OF ALL TYPE I OR II POSTS AND ALL SHEETING SIGNS SHALL BE INCIDENTAL TO THE PROJECT WITH NO ADDITIONAL PAYMENT BEING ALLOWED. ALL MATERIAL SHALL BE STORED IN ACCORDANCE WITH NOTE (8) ABOVE.
- (10) WHERE THE REMOVAL OF OVERHEAD STRUCTURE CONCRETE BASE IS CALLED FOR, THE BASE IS TO BE REMOVED TO A MINIMUM OF ONE FOOT (1') BELOW THE GROUND LINE, BACKFILLED TO EXISTING GROUND LINE, AND THE DISTURBED AREAS RESEDED.
- (11) WHERE THE REMOVAL OF BEAM SIGN SUPPORTS IS CALLED FOR, THE BEAM AND ANY CONCRETE PROJECTING ABOVE THE GROUND LINE ARE TO BE CUT OFF A MINIMUM OF ONE FOOT (1') BELOW EXISTING GROUND LINE OR THE ENTIRE BEAM AND CONCRETE BASE ARE TO BE REMOVED COMPLETELY AND BACKFILLED TO EXISTING GROUND LINE.
- (12) QUANTITY SHALL INCLUDE W-BARS AND ALL HARDWARE NECESSARY FOR ATTACHING SIGNS TO SUPPORTS.
- (13) QUANTITY IS ESTIMATED. THE EXACT NUMBER SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER. SEE SIGNING POSITIONING DETAIL SHEET FOR DELINEATOR PLACEMENT.

FILE NAME: P:\CIVIL\1471\SIGNS\SIGNING PLANS\T002005N.DGN

USER: b'reague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8,11,7,443

COUNTY OF	ITEM NO.	SHEET NO.
Campbell	6-2021.00	T02

SIGNING SPECIFICATION NOTES

THE FOLLOWING PUBLICATIONS ARE APPLICABLE TO THE WORK DESCRIBED HEREIN:

KENTUCKY DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (2004)
STANDARD HIGHWAY SIGNS -- FEDERAL HIGHWAY ADMINISTRATION
MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (2003 EDITION) -- FEDERAL HIGHWAY ADMINISTRATION

SCOPE OF WORK

TO FURNISH, FABRICATE AND ERECT IN PLACE ALL MATERIALS NECESSARY TO FORM COMPLETED SIGNS AS INDICATED AT LOCATIONS DESCRIBED ELSEWHERE IN THESE PLANS.

SIGN BASE MATERIAL

PANEL SIGNS:

PANEL SIGNS ARE TO BE FABRICATED FROM TWELVE INCH (12") WIDE ALUMINUM EXTRUSIONS AND, WHERE NOTED, COMPATIBLE SIX INCH (6") WIDE ALUMINUM EXTRUSIONS. WHEN A SIX INCH (6") EXTRUSION IS SPECIFIED, IT SHALL BE USED AS THE BOTTOM PANEL OF THE SIGN. TYPICAL CROSS-SECTIONS AND MINIMUM WEIGHTS PER FOOT ARE SHOWN ON THE MISCELLANEOUS DETAIL SHEET, COMPATIBLE SIDE EXTRUSIONS SHALL BE USED ON ALL SIGN EDGES. ALUMINUM MATERIAL FOR ALL EXTRUSIONS SHALL BE ALLOY 6063-T6 ASTM B221. ALL PORTIONS OF EXTRUSIONS WHICH ARE TO COMPOSE THE SIGN FACE SHALL BE PREPARED TO RECEIVE RETROREFLECTIVE BACKGROUND MATERIAL ACCORDING TO THE EXTRUSION AND RETROREFLECTIVE MATERIAL MANUFACTURER'S RECOMMENDATIONS. ALL REMAINING PORTIONS OF EXTRUSIONS (FRONT AND BACK) AND SIDE EXTRUSIONS ARE TO HAVE A SOFT MATTE FINISH. PANEL SIGNS SHALL BE LABELED AS P-#.

SHEET SIGNS:

SHEET SIGNS SHALL BE FABRICATED FROM EITHER 0.080 GAUGE OR 0.125 GAUGE ALUMINUM ALLOY 5052-H38 OR 6061-T6 SHEETS IN ACCORDANCE WITH ASTM B209 AND SHALL BE OF THE SIZE AND SHAPE SPECIFIED. THE SIDE OF THE SHEET TO BE USED AS THE SIGN FACE SHALL BE PREPARED TO RECEIVE RETROFLECTIVE BACKGROUND MATERIAL ACCORDING TO THE ALUMINUM SHHET AND RETRFLLECTIVE MATERIAL MANUFACTURER'S RECOMMENDATIONS. SHEETING SIGNS SHALL BE LABELED AS S-#.

SIGN MATERIALS

BACKGROUND MATERIAL:

SIGN SHEETING USED AS BACKGROUND MATERIAL FOR SIGN FACES IS TO BE THE COLOR SPECIFIED AND VISUALLY IN ACCORDANCE WITH STANDARD INTERSTATE COLORS. THIS MATERIAL (EXCEPT BLACK PORTIONS) SHALL BE RETROREFLECTORIZED AND MUST CONFORM TO THE REQUIREMENTS OF ASTM D 4956 FOR TYPE III SHEETING, AND SHALL MEET THE REQUIREMENTS OF SECTION 830 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

IN THE EVENT THAT GLASS BEAD ENCAPSULATED TYPE III SHEETING IS UTILIZED IT SHALL CONSIST OF:

RETROREFLECTIVE SHEETING HAVING AN INTEGRAL OR AIR CAVITY BETWEEN THE FRONT SURFACE AND THE OPTICAL ELEMENTS, MOUNTED ON AND FULLY COVERING ALUMINUM BASE COPY STOCK NOT OTHERWISE EMBOSSED OR CRIMPED BUT HAVING SUFFICIENT THICKNESS AND RIGIDITY TO PREVENT WARPING WHEN MOUNTED OR FASTENED TO THE SIGN PANEL.

ALL RETROREFLECTIVE MATERIALS SHALL BE FABRICATED AND ASSEMBLED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS AND/OR RECOMMENDATIONS.

LETTERS, SYMBOLS, AND BORDERS:

LETTER, SYMBOLS, AND BORDERS FOR PANEL SIGNS SHALL MEET REQUIREMENTS OF SECTION 830 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THIS MATERIAL SHALL BE RETROREFLECTORIZED AND MUST CONFORM TO ALL THE REQUIREMENTS OF ASTM D 4956 FOR TYPE VIII, OR IX SHEETING.

ALL ATTACHMENTS OF REMOVABLE COPY TO SIGN FACES SHALL BE MADE WITH "POP" FASTENERS ("POP" RIVETS). "POP" RIVETS SHALL BE OF THE PROTRUDING HEAD TYPE. BOTH THE RIVET AND MANDREL SHALL BE CORROSION RESISTANT TO THE MATERIAL IN WHICH IT IS INSERTED. COPY SHALL BE AFFIXED WITH A MINIMUM SIZE OF 1/8 INCH DIAMETER "POP" RIVETS, AND THE LENGTH SHALL BE AS NECESSARY TO PROPERLY APPLY COPY IN A WORKMANLIKE MANNER. PANEL OVERLAY SECTIONS SHALL BE AFFIXED WITH A "POP" RIVET WITH A MINIMUM DIAMETER OF 3/16 INCH, AND THE

LENGTH SHALL BE AS NECESSARY TO PROPERLYAPPLY COPY IN A WORKMANLIKE MANNER. ALL RIVETS SHALL BE APPROVED BY THE ENGINEER PRIOR TO COMMENCING WORK ON THE PROJECT.

ROUTE MARKERS:

ROUTE MARKERS FOR PANEL SIGN MOUNTING ONLY, ARE TO BE A RETROREFLECTORIZED WHITE CUT-OUT OF THE U.S. ROUTE SHIELD OR KENTUCKY ROUTE SHIELD, OMITTING THE BLACK BACKGROUND ON THE STANDARD RECTANGULAR SHAPES. BORDERS ARE NOT TO BE USED ON THE CUT-OUT SHAPES AND THEIR DIMENSIONS ARE TO BE AS SHOWN IN THE STANDARD HIGHWAY SIGNS MANUAL. ROUTE MARKERS ARE TO BE SPACED EVENLY ACROSS THE PANEL SIGN FACE.

ROUTE MARKERS FOR PANEL SIGNS SHALL MEET THE REQUIREMENTS OF SECTION 830 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THIS MATERIAL SHALL BE RETROREFLECTORIZED AND MUST CONFORM TO THE REQUIREMENTS OF ASTM D 4956 FOR TYPE III SHEETING. THE SHEETING SHALL BE MOUNTED ON ALUMINUM BASE COPY STOCK WITH A MINIMUM THICKNESS OF 0.080 INCHES.

DESTINATION-DIRECTION SIGNS:

DESTINATION-DIRECTION SIGNS ARE TO BE OF THE SIZE INDICATED, AND SHALL HAVE SILVER/WHITE LETTERS, SYMBOLS, AND BORDERS. THIS MATERIAL SHALL BE RETROREFLECTORIZED AND MUST CONFORM TO THE REQUIREMENTS OF ASTM D 4956 FOR TYPE III SHEETING, AND SHALL MEET THE CONDITIONS OF SECTION 830 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. (SHEETING SIGNS ONLY)

SIGN MESSAGES:

SIGN MESSAGES SHOWN ARE ULTIMATE MESSAGES. DUE TO PARTIAL CONSTRUCTION, IT MAY BE NECESSARY TO MAKE CHANGES IN SOME OF THESE MESSAGES. THESE CHANGES WILL BE DETERMINED BY THE ENGINEER. SHOULD A SIGN CHANGE BE DETERMINED BEFORE THE SIGN IS INSTALLED, THE ULTIMATE MESSAGE COPY WILL BE STORED ON THE PROJECT BY THE CONTRACTOR. SHOULD A SIGN CHANGE BE DETERMINED AFTER INSTALLATION, STATE FORCES WILL REMOVE THE ULTIMATE MESSAGE. COPY WILL BE STORED BY THE STATE UNTIL APPLICABLE, AT WHICH TIME IT WILL BE INSTALLED BY STATE FORCES. ANY COPY NEEDED FOR A TEMPORARY MESSAGE WILL BE SUPPLIED AND INSTALLED BY THE STATE.

HARDWARE:

ALL HARDWARE FOR THE ASSEMBLY OF PANEL SIGNS AND THE ATTACHMENT OF THESE SIGNS TO THEIR SUPPORTS SHALL BE AS RECOMMENDED BY THE PANEL MANUFACTURER. PLACEMENT OF POST CLIP SHALL BE AS SHOWN ON THE SIGNING MISCELLANEOUS DETAIL SHEET.

ALL HARDWARE FOR THE ERECTION OF SHEETING SIGNS SHALL BE CADMIUM PLATED STEEL IN ACCORDANCE WITH ASTM B-776 AND ASTM A-307.

GROUND-MOUNTED SIGN SUPPORTS

GENERAL:

ALL SIGNS SHALL BE POSITIONED AS SHOWN ON THE POSITIONING DETAIL SHEET. ALL BEAMS AND POSTS SHALL BE OF SUFFICIENT LENGTHS TO EXTEND FROM THE TOP OF THE SIGN TO THE REQUIRED BASE EMBEDMENT.

BEAMS:

ALL BEAMS SHALL BE EITHER TYPE "A" (STANDARD BEAM INSTALLATION), OR TYPE "C" (BREAKAWAY SIGN POST SUPPORT SYSTEM INSTALLATION). TYPE "A" BEAMS ARE SHOWN ON THE PANEL SIGN DETAIL SHEET, AND THE TYPE "C" BEAMS ARE SHOWN ON THE BREAKAWAY SIGN SUPPORT SYSTEM FOR "C" BEAM SHEET. APPROVED MANUFACTURERS FOR BREAKAWY (TYPE "C") BEAM SUPPORT SYSTEMS HAVE BEEN PLACED ON THE LIST OF APPROVED MATERIALS. THE TYPE AND SIZE OF BEAM TO BE USED SHALL BE INDICATED FOR EACH PANEL SIGN ON THE SIGN DETAIL SHEETS. BEAM LENGTHS INCLUDED IN THESE PLANS ARE FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR SHALL TAKE FIELD MEASUREMENTS AT EACH SIGN LOCATION AND CROSS SECTIONS SHALL BE DEVELOPED TO VERIFY BEAM LENGTHS, WITH ANY DISCREPANCIES BROUGHT TO THE ATTENTION OF THE ENGINEER FOR RESOLUTION.

BEAMS SHALL BE A-36 STEEL GALVANIZED IN ACCORDANCE WITH ASTM A-123, CURRENT EDITION.

STEEL POSTS:

TYPE I STEEL POSTS SHALL BE EITHER STANDARD INSTALLATION IN SOIL, WITH SOIL STABILIZER, OR TYPE "D" (BREAKAWAY SIGN POST SUPPORT SYSTEM INSTALLATION). APPROVED MANUFACTURERS FOR BREAKAWAY (TYPE "D") POST SYSTEMS HAVE BEEN PLACED ON THE LIST OF APPROVED MATERIALS. BRACING, IF REQUIRED, SHALL BE INCIDENTAL TO TYPE I POST.

TYPE II POST SHALL BE STANDARD INSTALLATION IN SOIL, WITH A SOIL STABILIZER. INSTALLATION PROCEDURES AND BRACING REQUIREMENTS ARE DETAILED ON THE SHEETING SIGN DETAIL SHEET.

ALL STEEL POST SHALL MEET THE REQUIREMENTS OF SECTION 832 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION WITH THE EXCEPTION THAT TYPE I POST SHALL BE PROVIDED IN THE FOLLOWING SIZES:

Outside Dimensions (in.)	Corner Radii (in.)	Wall Thickness Gauge (in.)	Weight (lb/ft)
2 by 2	5/32	(.105) 12	2.42
2 1/2 by 2 1/2	5/32	(.105) 12	3.14
2 1/2 by 2 1/2	5/32	(.135) 10	4.01

SIGNING SPECIFICATION NOTES

MILEPOST MARKERS

MILEPOST MARKERS SHALL CONFORM TO THE GENERAL REQUIREMENTS SET FORTH IN SECTION 2D-45 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. ADDITIONAL REQUIREMENTS ARE GIVEN ON THE SIGNING POSITIONING DETAIL SHEET.

SIGN PANELS ARE TO BE FABRICATED FROM 0.080 GAUGE ALUMINUM ALLOY 5052-H38 SHEET IN ACCORDANCE WITH ASTM B209, AND SECTION 833 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

THE SIGN PANELS SHALL BE PROPERLY PREPARED TO RECEIVE THE RETROREFLECTIVE BACKGROUND MATERIAL ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS. DIMENSIONS FOR ONE, TWO, AND THREE DIGIT SIGNS ARE SHOWN ON THE SIGNING POSITIONING DETAIL SHEET.

BACKGROUND MATERIAL SHALL BE STANDARD INTERSTATE GREEN IN COLOR AND SHALL BE RETROREFLECTORIZED. COPY IS TO BE SILVER/WHITE RETROREFLECTIZED, TEN INCH (10") SERIES "C" NUMERALS OF THE CUT-OUT, NONREMOVABLE TYPE. BOTH BACKGROUND AND COPY MATERIAL MUST MEET TYPE III, CLASS "I" REQUIEMENTS OF SECTION 830 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

MINIMUM LENGTHS OF POST SHALL BE TEN FEET (10') WHEN USED WITH ONE DIGIT MARKER, ELEVEN FEET (11') WITH TWO DIGIT MARKER, AND TWELVE FEET (12') WITH THREE DIGIT MARKER. POSTS SHALL BE DRIVEN AND SIGN PANELS MOUNTED TO MAINTAIN FOUR FEET (4') VERTICAL CLEARANCE FROM THE ELEVATION OF THE NEAREST EDGE OF ROADWAY PAVEMENT TO THE BOTTOM OF THE SIGN FACE.

FINAL LOCATION OF MILEPOST MARKERS SHALL BE VERIFIED BY TRIMARC. NOTIFY THE FOLLOWING REPRESENTATIVE OF TRIMARC, AT LEAST TWO WEEKS IN ADVANCE OF BEGINNING WORK ON THIS ITEM:

TODD HOOD
901 WEST MAIN STREET
LOUISVILLE, KY 40202

502-587-6624
270-307-7456

INTERCHANGE RAMP DESIGN IS SUFFICIENTLY VARIED THAT NO SINGLE DELINEATOR SPACING CAN FIT EVERY SITUATION; THEREFORE, THE TYPICAL DELINEATION AND SPACING VALUE TABLE SHOWN ON THE POSITIONING DETAIL SHEET AND SECTION 3D-04 OF THE 2003 MUTCD SHOULD BE EMPLOYED AS A GUIDE TO DELINEATOR PLACEMENT ON INTERCHANGE RAMPs.

LATERAL AND VERTICAL CLEARANCES ARE SHOWN ON THE POSITIONING DETAIL SHEET. INSTALLATION OF DELINEATORS ON CROSSROADS SHALL BE LIMITED TO DECELERATION AND ACCELERATION LANES SERVING MAIN LINE RAMPs.

NEW CONCRETE BASES, SUPPORT BEAMS, ETC. ARE TO BE INSTALLED PRIOR TO DISMANTLING ANY EXISTING SIGN. IF ANY EXISTING SIGNS ARE TO BE OUT OF SERVICE FOR MORE THAN ONE WORK SHIFT, TEMPORARY SIGNING OF THE PROPER SHAPE, AND WITH COPY OF SIMILAR CONFIGURATION TO THE EXISTING SIGNING SHALL BE INSTALLED AT THE SAME APPROXIMATE STATION AS THE OUT-OF-SERVICE SIGN. THE COST OF ANY TEMPORARY SIGNING SO USED SHALL BE INCIDENTAL TO THE COST OF REMOVAL OF EXISTING SIGN SUPPORT BEAMS.

THE REMOVAL OF BEAM SIGN SUPPORTS IS TO BE DONE CONCURRENTLY WITH THE RELOCATION OF AFFECTED SIGNS TO NEW SUPPORTS.

IF A MANUFACTURER'S WARRANTY IS FURNISHED TO THE CONTRACTOR ON ANY MATERIALS COVERED UNDER THESE SPECIFICATIONS, THE SAME WARRANTY SHALL BE FURNISHED TO THE STATE BY THE CONTRACTOR.

ALL SIGNS ARE TO BE LOCATED AT THE APPROXIMATE STATIONS LISTED AND THE EXACT LOCATION FOR EACH SIGN SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER; HOWEVER, IF AN SIGN IS RELOCATED MORE THAN TWENTY-FIVE FEET (25') FROM THE STATION LISTED, THE NEW LOCATION MUST BE APPROVED BY THE DIVISION OF HIGHWAY DESIGN AT (502) 564-3280.

ON SHEETING SIGNS WHERE THERE ARE MORE THAN ONE SIGN ASSEMBLY MOUNTED BEDSIDE EACH OTHER, THE POST SHALL BE SPACED TO PROVIDE APPROXIMATELY SIX INCHES (6") OF SPACING BETWEEN SIGNS.

CLEARING AND GRUBBING, AND TREE TRIMMING, WHEN REQUIRED FOR CONSTRUCTION OF THE SIGN PANELS, WILL BE INCIDENTAL TO THE CONTRACT AND NO DIRECT PAYMENT WILL BE ALLOWED.

SIGN COVERING IS NOT RECOMMENDED. HOWEVER, IF IT IS ABSOLUTELY NECESSARY TO COVER THE SIGN FACE TEMPORARILY FOLLOWING ERECTION, USE CAUTION SINCE SOME COVERINGS MAY CAUSE PERMANENT DAMAGE TO THE SIGN FACE FOLLOWING EXPOSURE TO MOISTURE, SUNLIGHT, ETC. POROUS CLOTH OR GEOTEXTILE FABRIC COVERS WHICH ARE FOLDED OVER THE SIGN EDGES AND SECURED AT THE BACK OF THE SIGN HAVE BEEN USED SUCCESSFULLY FOR LIMITED PERIODS. DO NOT USE TAPE, PAPER, PLASTIC, OR SHEET METAL COVERS. ANY SIGNS THAT ARE DAMAGED AS A RESULT OF COVERING SHALL BE REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE DEPARTMENT.

TYPE I AND II STEEL POST IN SOIL SHALL BE DRIVEN FOUR FEET (4') BELOW THE GROUND LINE AS SHOWN. HOWEVER, IF SOILD ROCK IS ENCOUNTERED THE CONTRACTOR SHALL DRILL HOLES OF THE REQUIRED DEPTH INTO THE ROCK, AND BACKFILL WITH CONCRETE. THE COST SHALL BE INCIDENTAL TO STEEL POST, AND SOIL STABILIZERS WILL NOT BE REQUIRED.

ANY AREA DISTURBED SHALL BE SIDE GRADED TO THE EXISTING SLOPES AND RESEEDED AS DIRECTED BY THE ENGINEER, AND AT NO ADDITIONAL COST TO THE DEPARTMENT.

MEDIAN CROSSOVER SIGN

THE CONTRACTOR SHALL INSTALL 48" X 48", "NO U TURN" SIGNS (R3-4) AT EACH MEDIAN CROSSOVER. THIS IS TO BE DONE WHETHER ALL NEEDED INSTALLATIONS ARE MENTIONED IN THE FOLLOWING SHEETS OR NOT. AT CROSSOVERS ON MEDIANS SIXTY FEET (60') WIDE AND UNDER, THE SIGNS SHALL BE MOUNTED PERPENDICULAR TO THE ROADWAY ON THE SAME POSTS IN THE CENTER OF THE MEDIAN, ONE FACING TRAFFIC IN EACH DIRECTION. AT CROSSOVERS ON MEDIANS OVER SIXTY FEET (60') WIDE, THE SIGNS SHALL BE MOUNTED PERPENDICULAR TO THE ROADWAY ON SEPARATE POSTS AT THE MEDIAN SHOULDER ON THE FAR SIDE OF THE CROSSOVER, ONE FACING TRAFFIC IN EACH DIRECTION.

CONCRETE BASES

ALL CONCRETE BASES SHALL BE OF CLASS "A" CONCRETE FOR SIGNS AND SHALL BE AS SHOWN ELSEWHERE IN THESE PLANS.

EXCAVATION NECESSARY TO CONSTRUCT BASES AND FOOTINGS IS INCIDENTAL TO THE COST OF CLASS "A" CONCRETE FOR SIGNS.

SAMPLES, TESTING, ETC.

BEFORE BEGINNING INSTALLATION, THE CONTRACTOR SHALL FURNISH TO THE RESIDENT OR PROJECT ENGINEER FOR WRITTEN APPROVAL DRAWINGS, DESCRIPTIONS, MANUFACTURER'S CUTS ETC. COVERING ALL MATERIALS TO BE USED. MILL TEST REPORTS FOR BEAMS, STEEL PANELS, AND EACH DIFFERENT GAUGE OF ALUMINUM OR STEEL SHEETING USED MUST BE SUBMITTED TO THE DIVISION OF CONSTRUCTION AND APPROVED PRIOR TO ERECTION.

MISCELLANOUS

RIGHT IS RESERVED TO INSPECT FABRICATION AND ERECTION WORK, AN INSPECTION (DAY AND NIGHT) WILL BE MADE AFTER COMPLETION OF INSTALLATION TO DETERMINE IF THE INTENT OF THE SPECIFICATIONS IS SATISFIED.

FILE NAME: P:\CIVIL\1471\SIGNS\SIGNING PLANS\T00300SN.DGN

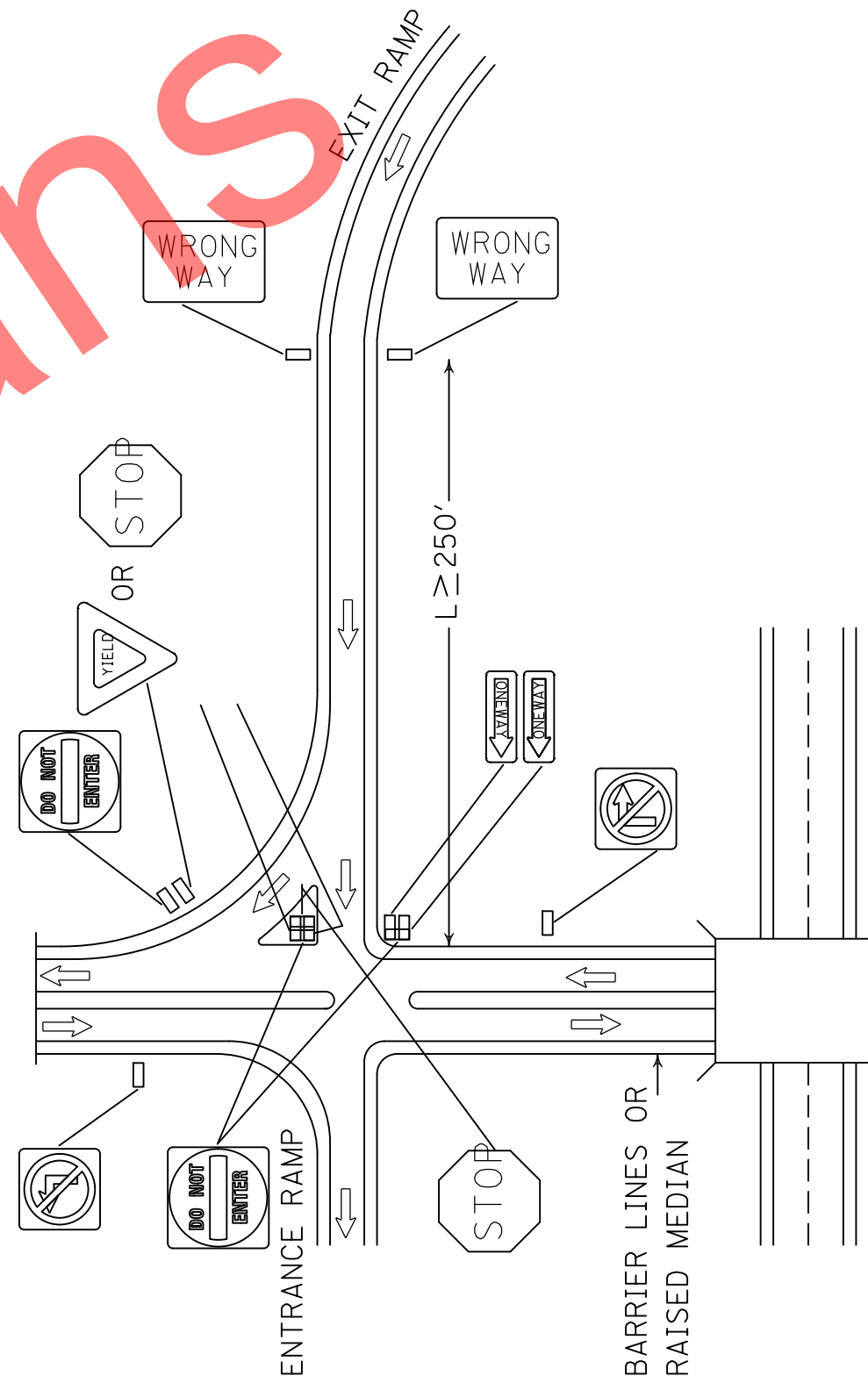
USER: b'reague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8,11,7,443

SPECIAL NOTES AND DETAIL

- NEW SIGNS ARE TO BE INSTALLED AT EXISTING LOCATIONS UNLESS OTHERWISE NOTED ON THE PLANS.
- EXISTING I-BEAMS ON WHICH SHEETING SIGNS ARE ATTACHED SHALL BE REMOVED AND REPLACED WITH TYPE II POSTS, UNLESS THEY ARE LOCATED BEHIND GUARDRAIL.
- DO NOT REMOVE OR DISTURB SUCH SIGNS AS "KEEP/RIGHT/EXCEPT/TO PASS", "\$500/FINE/FOR/LITTERING" OR "EXTENDED WEIGHT LIMIT" SIGNS.
- REMOVE AND DO NOT REPLACE THE WHITE ON BLUE GENERAL SERVICES SIGNS AT THE EXIT RAMP TERMINALS, "EMERGENCY STOPPING ONLY" SIGNS, AND THE ROUTE MARKER THAT IS LOCATED INSIDE THE INTERCHANGE PAST THE EXIT GORE AREA.
- ADVISORY EXIT SPEED SIGNS (W13-2) SHOULD BE POSTED ALONG THE DECELERATION LANE SO THEY WILL BE VISABLE FROM A SUFFICIENT DISTANCE.
- ALL EXISTING SHEETING SIGNS WITHIN THE LIMITS OF THIS PROJECT ARE TO BE REMOVED FROM THE POSTS AND REPLACED WITH NEW IDENTICAL SIGNS, AS SHOWN ON THE SIGNING PLANS, UNLESS OTHERWISE SPECIFIED. HOWEVER, ANY POSTS THAT ARE DAMAGED, SPLICED, OR DO NOT CONFORM TO THE MOUNTING SPECIFICATIONS SHOWN ON THE "POSITIONING DETAIL SHEET" MUST BE REPLACED WITH NEW POSTS OF THE SAME TYPE AND SIZE AS DIRECTED AND APPROVED BY THE ENGINEER. THE REMOVED SIGNS AND POSTS SHALL BECOME THE PROPERTY OF THE CONTRACTOR. THE COST FOR REMOVING EXISTING SHEETING SIGNS AND POSTS, RESETTling EXISTING POSTS, RELOCATING EXISTING POSTS AND SIGNS TO CONFORM TO THE SIGNING PLANS AND SPECIFICATIONS, AND ALL HARDWARE REQUIRED TO ATTACH THE NEW SIGNS TO THE EXISTING SUPPORTS SHALL BE INCIDENTAL TO THE PROJECT. IN ADDITION TO REPLACING ALL EXISTING SIGNS, ALL SIGNS SHOWN ON THE SIGNING PLANS MUST BE INSTALLED. FOR SIGN SIZES AND SPECIFICATIONS, SEE THE CURRENT PROJECT SIGNING PLANS DETAIL SHEETS.
- IN THE GORE AREAS WHERE NEW EXIT GORE SIGNS AND BEAMS ARE TO BE CONSTRUCTED, ANY EXISTING CONCRETE AND STUB PROJECTIONS SHALL BE REMOVED OR CUT OFF ONE FOOT BELOW THE GROUND LINE. THE COST SHALL BE INCIDENTAL TO THE PROJECT.
- THE COST FOR ANY SIGN BRACKETS AND ANY OTHER HARDWARE REQUIRED TO ATTACH NEW SIGNS ON EXISTING TRUSSES AND CANTILEVERS SHALL BE INCLUDED IN THE UNIT PRICE OF SIGN BASE MATERIAL FOR PANEL SIGNS.
- DUPLICATE SIGNS SHALL NOT BE DISPLAYED DURING THE CONSTRUCTION OF THIS PROJECT.
- SHEETING SIGNS (D1-1, D1-2, D1-3) EQUAL OR LESS THAN 72" X 42" REQUIRE TWO TYPE II POSTS.
- REPLACE ALL SIGNS SUCH AS "NO U TURN" (R3-4), 48" X 48", S.B.M. (.125 OR .12), "DEER/CROSSING" (W11-3), 48" X 48", S.B.M. (.125 OR .12), "BRIDGES/FREEZE/BEFORE/ROADWAY" BLACK ON YELLOW, 48" X 48", (.125 OR .12), AND "MILE POST MARKERS". SEE SIGN SPECIFICATION SHEET FOR DETAILS.
- THE COST FOR REMOVING EXISTING PANEL SIGNS SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
- THE COST FOR REMOVING LIGHT FIXTURES FROM EXISTING SIGNS SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF ANY TREES THAT OBSCURE THE SIGNS, AS DIRECTED BY THE ENGINEER. THE COST SHALL BE INCIDENTAL TO THE PROJECT.
- THE EXIT NUMBER SIGN ON ALL EXISTING SIGNS SHOWN ON THESE PLANS SHALL BE REPOSITIONED TO THE RIGHT OF THE SIGN AS SHOWN ON THE MISCELLANEOUS DETAIL SHEET, WITH THE EXCEPTION OF EXIT NUMBER SIGNS NOTED.
- THE HORIZONTAL CLEARANCE "X" ON ALL THE SHEETING SIGNS SHOULD BE A MINIMUM OF 6 FT. WHERE POSSIBLE, AS SHOWN ON THE SIGNING POSITIONING DETAIL SHEET.
- IF ANY SIGN IS LOCATED NEAR A LUMINAIRE OR ANOTHER POLE, IT SHALL BE INSTALLED IN ADVANCE OF THE POLE SO THAT THE MOTORISTS VIEW OF THE SIGN WILL NOT BE OBSTRUCTED.
- THE SIGNS THAT DO NOT COMPLY WITH THE "MUTCD" SHALL BE REMOVED BY THE CONTRACTOR AS APPROVED AND DIRECTED BY THE ENGINEER.



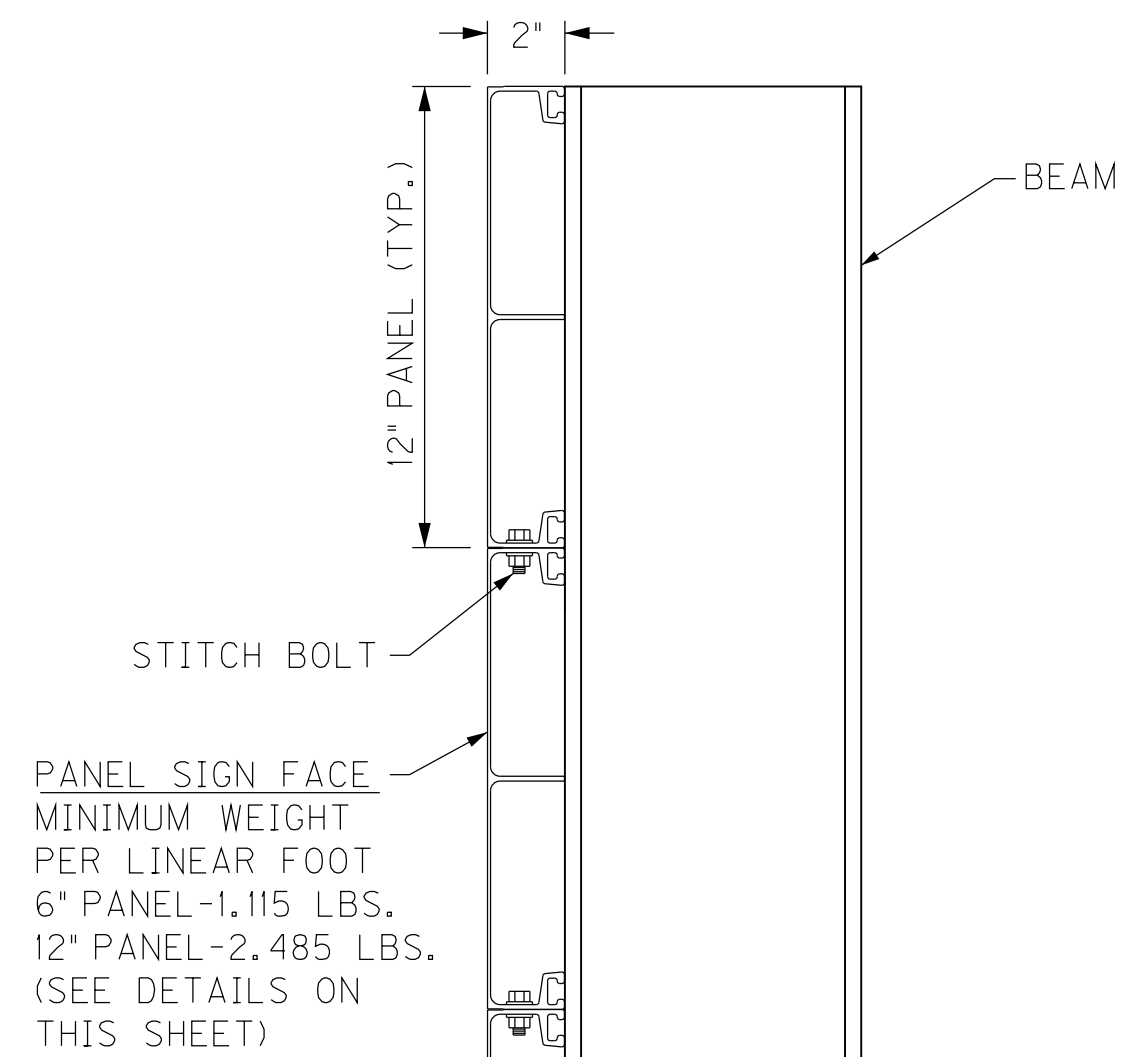
TYPICAL REGULATORY SIGNING AT RAMP TERMINALS
NOT TO SCALE

FILE NAME: P:\CIVIL\1471\SIGNS\SIGNING PLANS\T00600SN.DGN

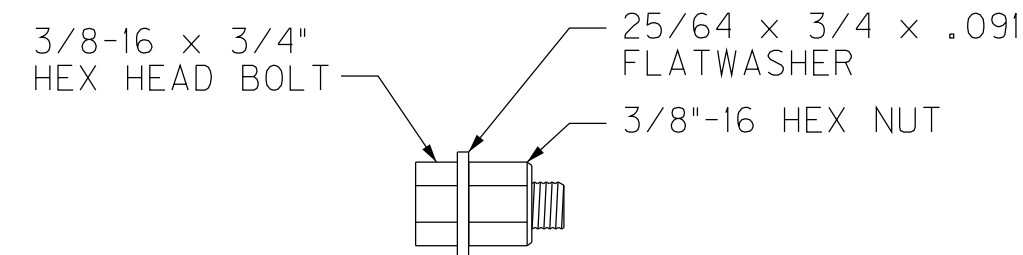
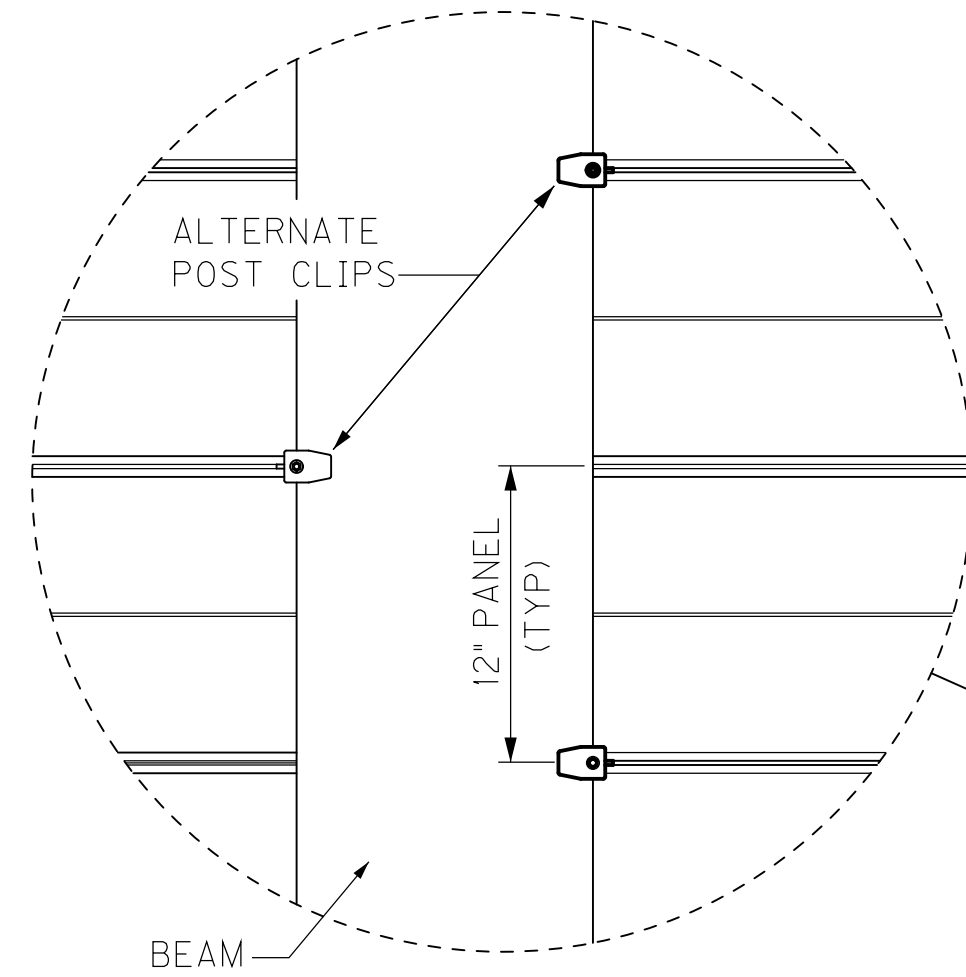
USER: b'reague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8.11.7.443

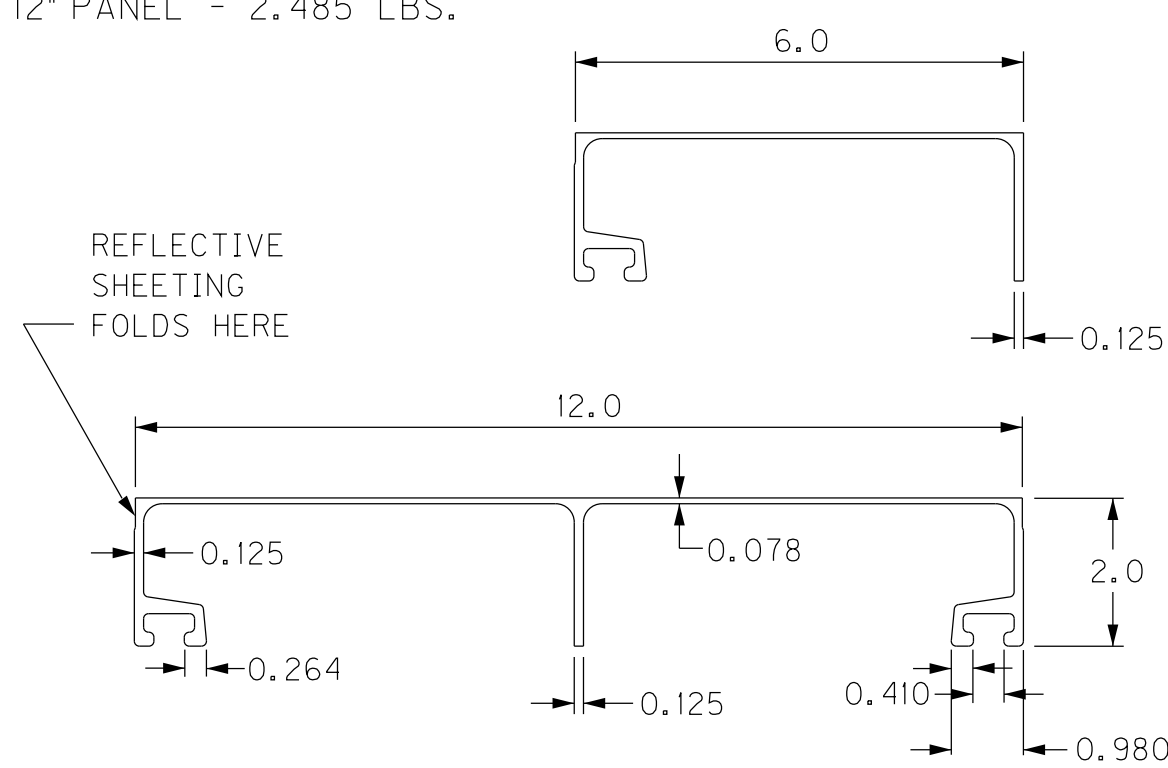


SECTION "A-A"

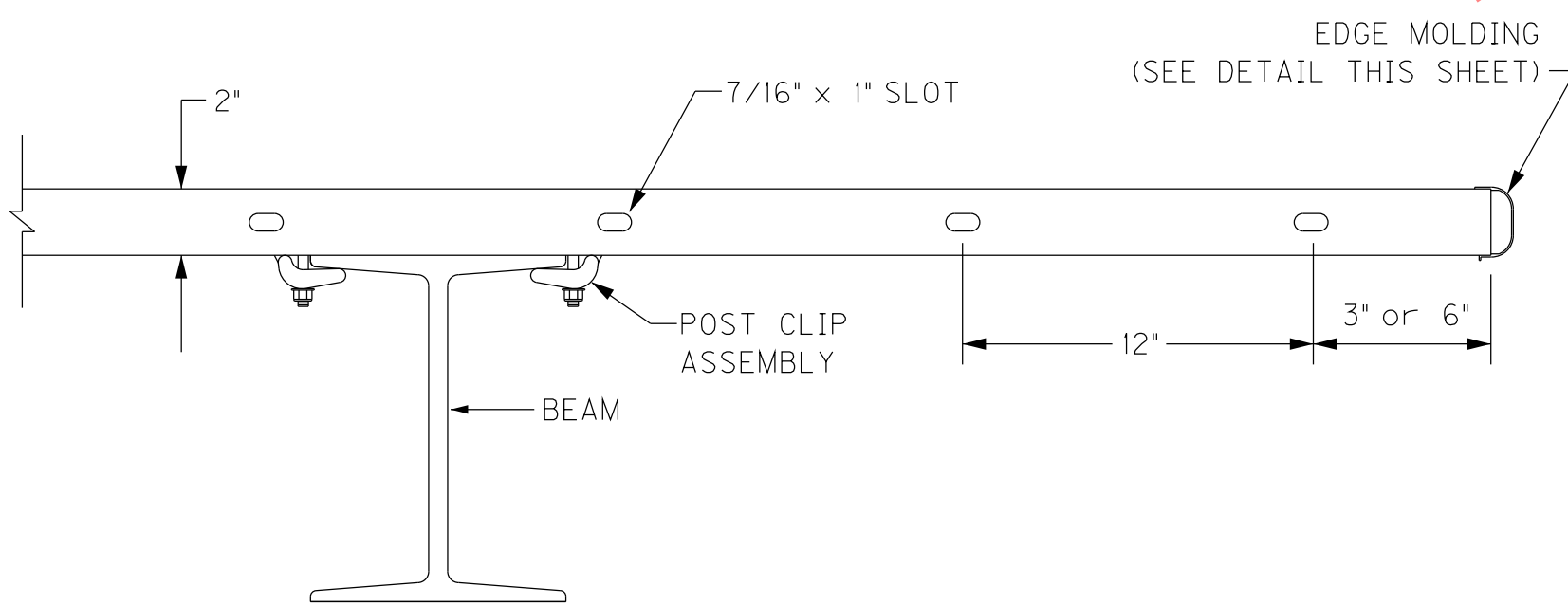


STITCH BOLT DETAIL

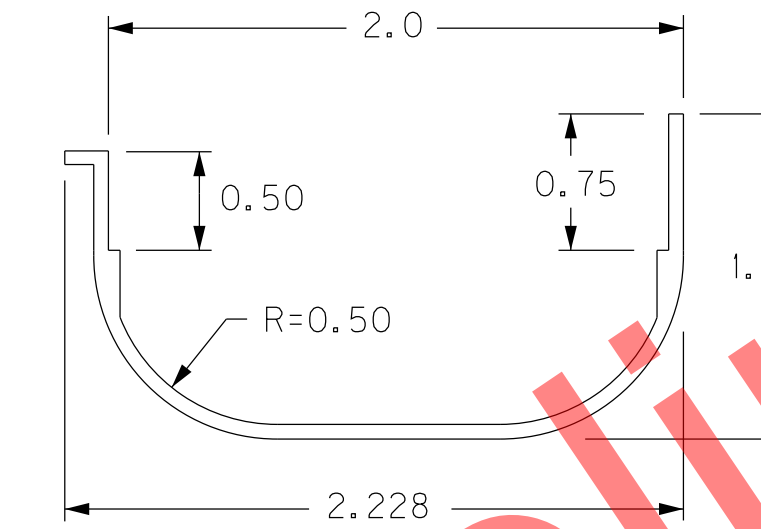
MINIMUM WEIGHT PER LINEAR FOOT
6" PANEL - 1.115 LBS.
12" PANEL - 2.485 LBS.



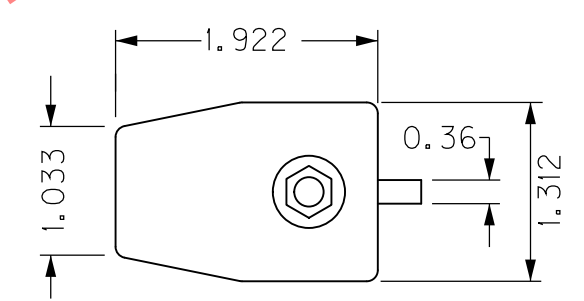
ALUMINUM PANEL DETAILS



TOP VIEW OF PANEL SIGN

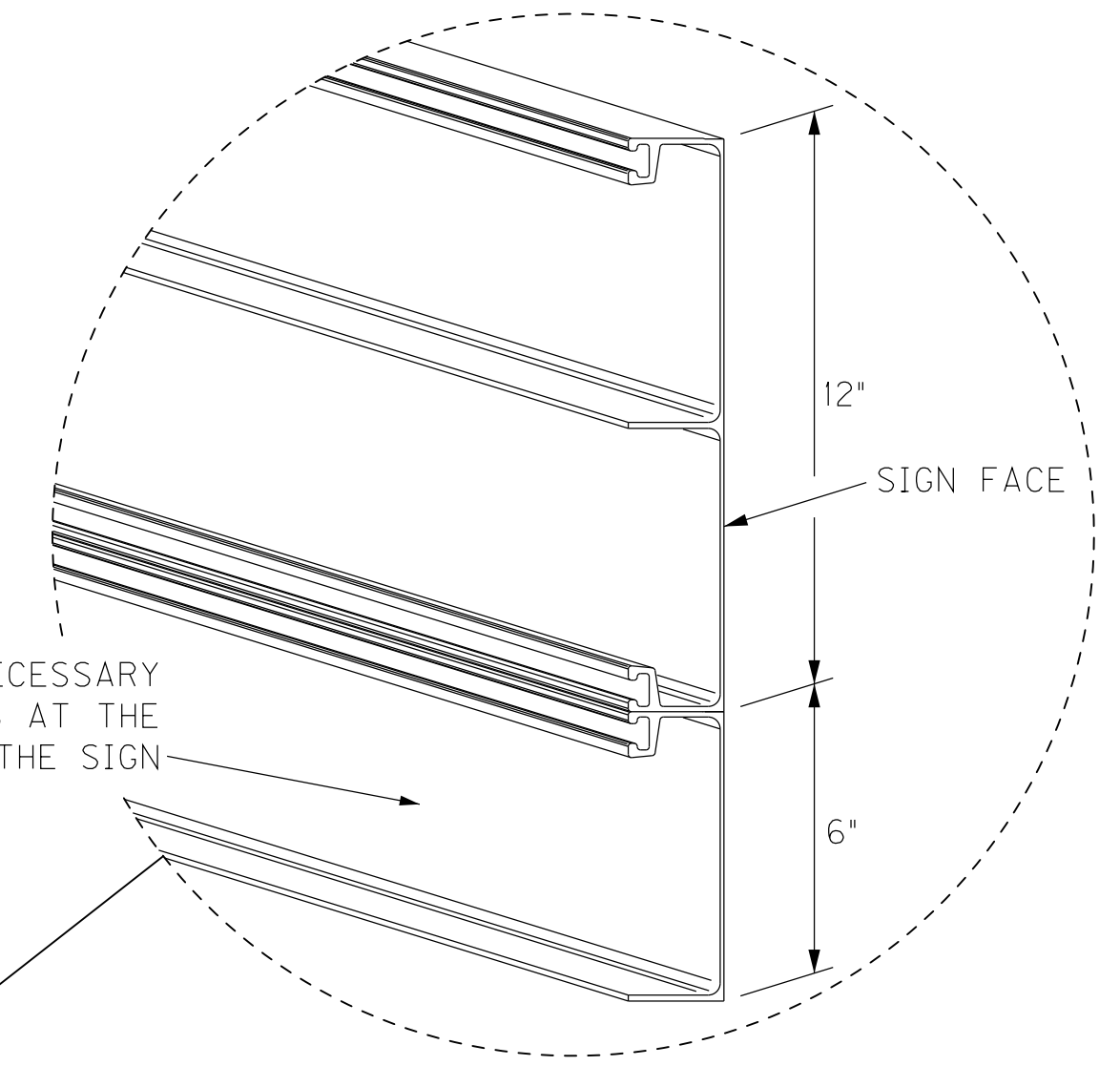
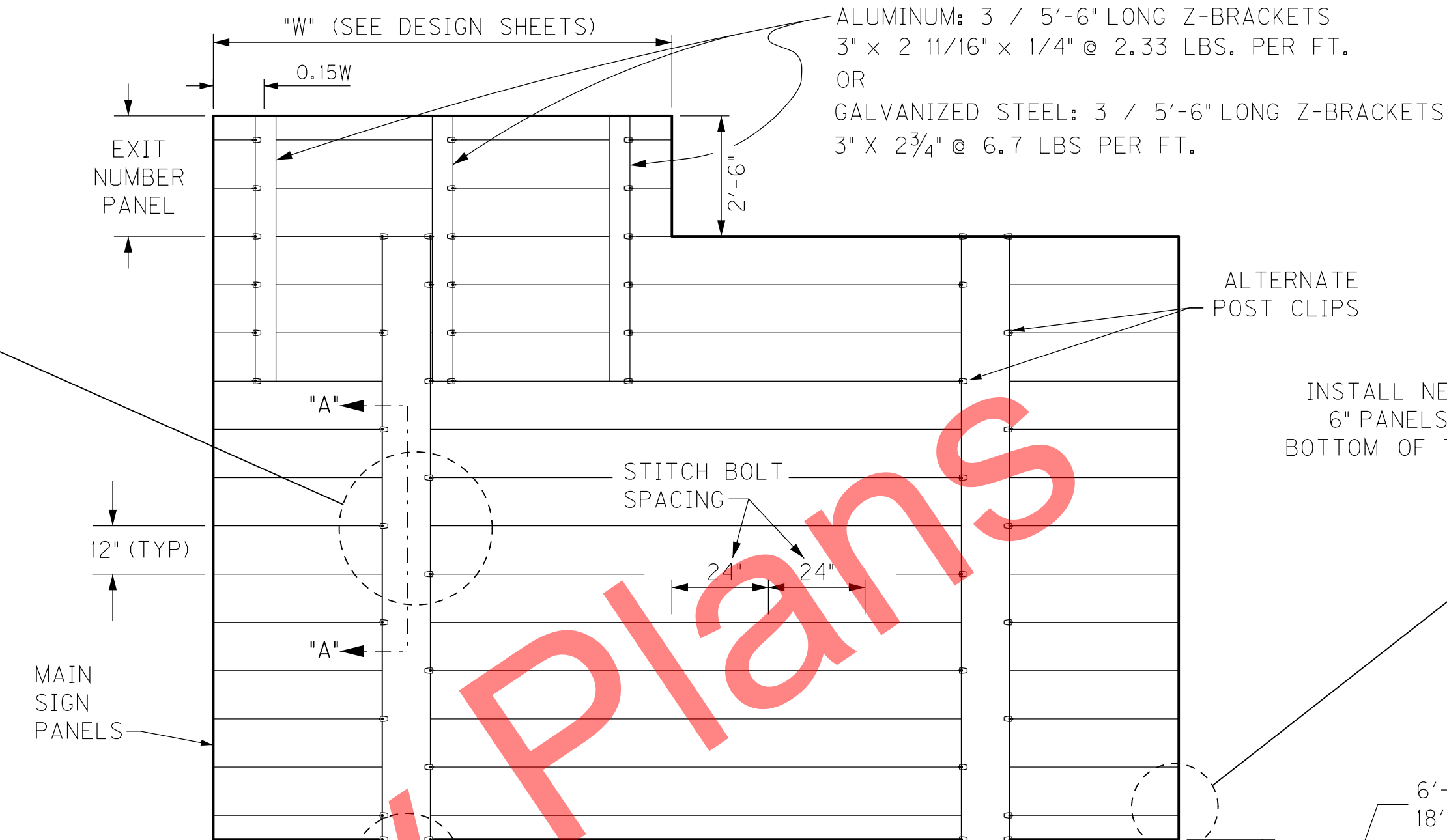


EDGE MOLDING DETAIL

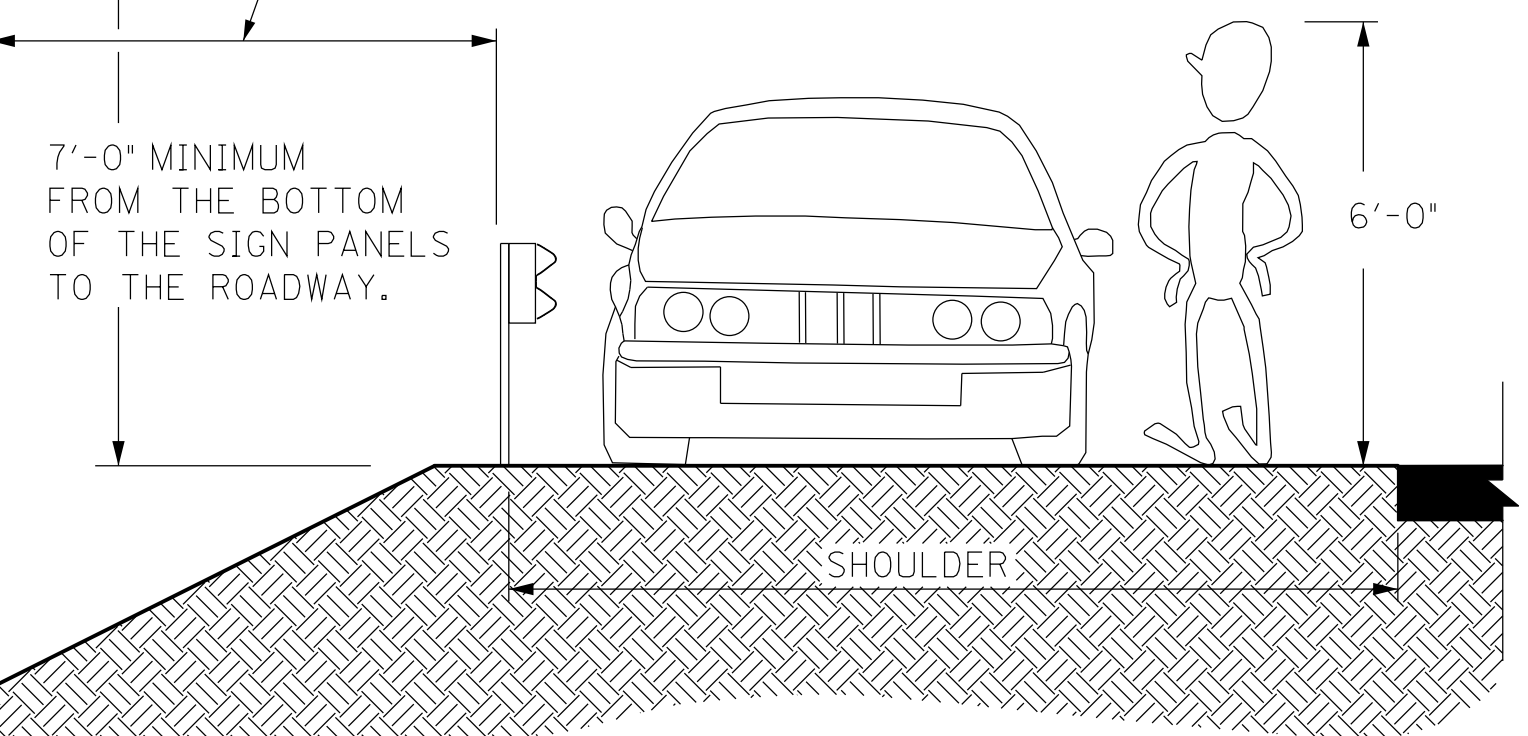


POST CLIP ASSEMBLY

NOTE:
THE COST FOR ATTACHING EXIT NUMBER SIGN SHALL BE INCLUDED IN THE BID ITEM FOR SIGN
BASE MATERIAL FOR PANEL SIGNS AND SHALL INCLUDE ALL Z-BRACKETS AND HARDWARE.
THE EXIT NUMBER SIGN SHALL BE CENTERED OVER THE LEFT OR RIGHT SIDE OF SIGN AS SHOWN
ON THE PLANS.



6'-0" IF PROTECTED BY GUARDRAIL (TYPE "A")
18'-0" IF NOT PROTECTED BY GUARDRAIL (TYPE "C")



(SEE DETAIL SHEETS FOR MORE INFORMATION)

TYPE "A" FIXED SUPPORT
BEAM SHALL BE ONE
PIECE FROM THE TOP OF
THE SIGN TO THE BOTTOM
OF THE CONCRETE

NOTE:
SHOULD THE CONTRACTOR OVERDRILL THE HOLES, THE EXTRA
CONCRETE WILL BE AT THE EXPENSE OF THE CONTRACTOR.
PAYMENT WILL BE DETERMINED BY THE DIMENSIONS SHOWN
ON THE DESIGN SHEETS.

THE TOPS OF THE BASES ARE TO BE FINISHED TO ASSURE THAT
THERE IS ADEQUATE DRAINAGE AWAY FROM THE CENTER OF
THE BASE.

SEE SIGN DESIGN SHEETS FOR "a" AND "b" DIMENSIONS

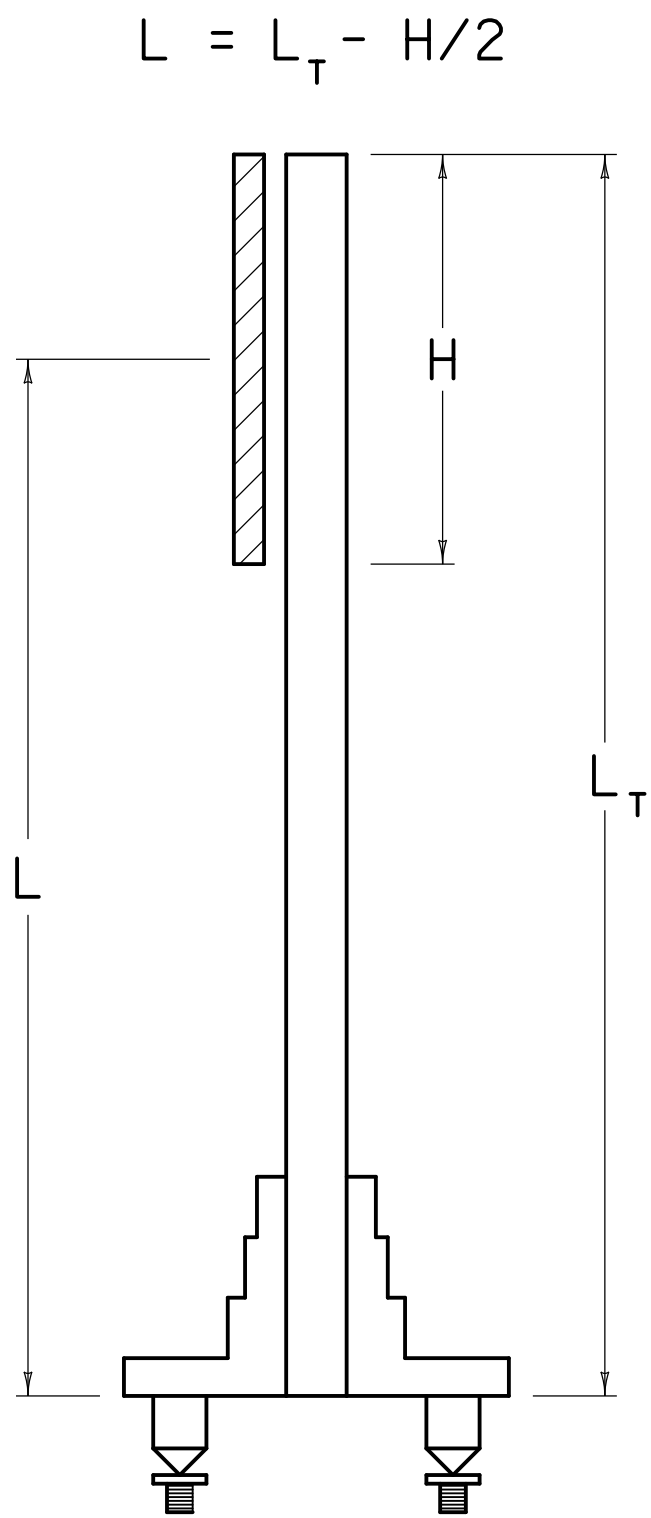
4" MAXIMUM
CLEARANCE ABOVE
THE GROUND LINE
(SEE NOTE #7 ON THE
TYPE "C" BREAKAWAY
FOOTING DETAIL SHEET

NOT TO SCALE

PANEL SIGN DETAILS SHEET

~NOTES~

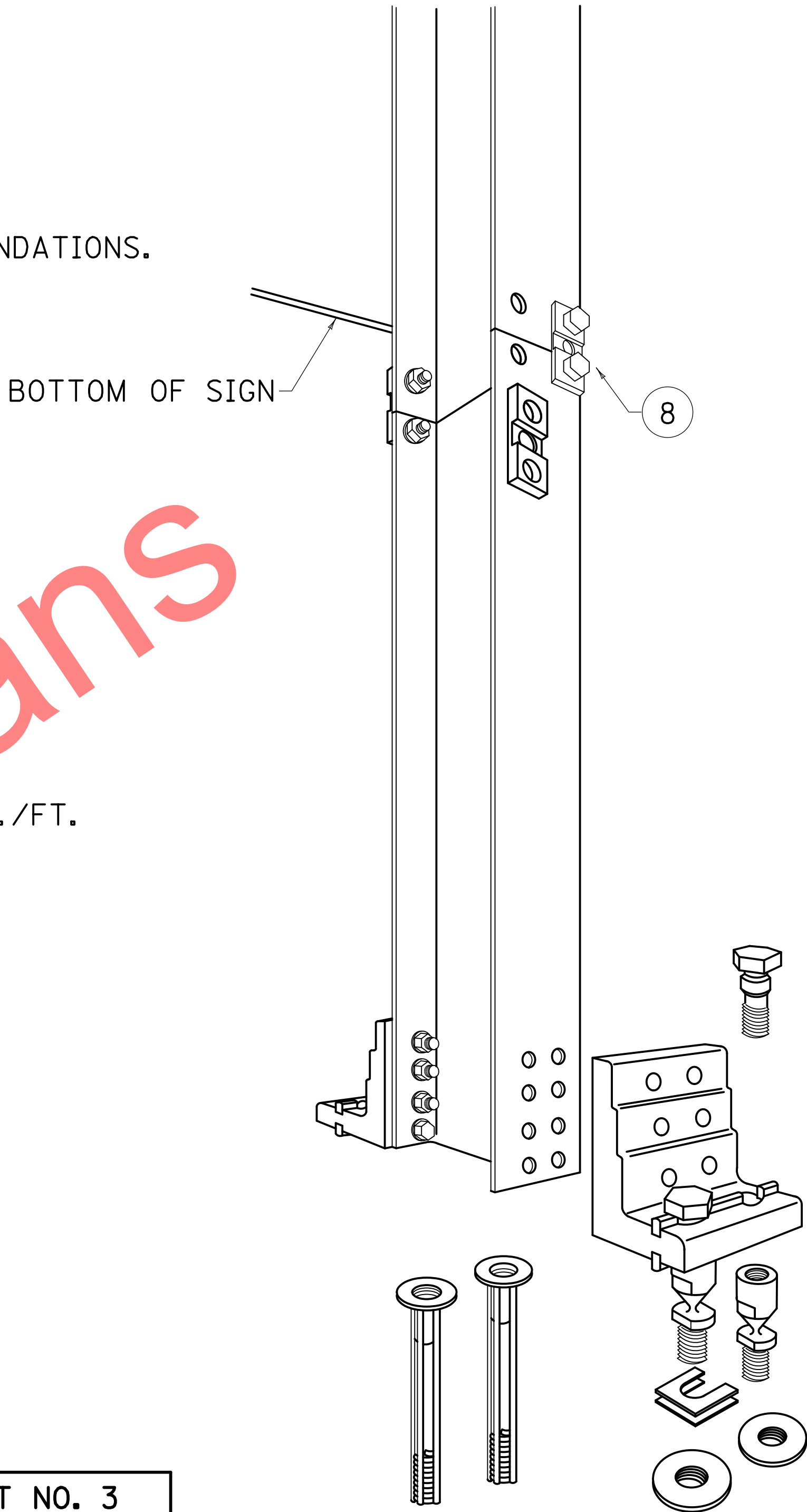
1. BREAKAWAY SIGN SUPPORT SYSTEM FOR TYPE C BEAM SHALL BE SELECTED FROM THE KENTUCKY DEPARTMENT OF HIGHWAYS APPROVED LIST FOR BREAKAWAY SIGN SUPPORT SYSTEMS OR AN APPROVED EQUAL. ACCEPTABLE ALTERNATE BREAKAWAY SIGN SUPPORT SYSTEMS SHALL BE APPROVED BY THE DIVISION OF HIGHWAY DESIGN AND FHWA PRIOR TO INSTALLATION.
2. SELECTION OF THE PROPER BRACKET NUMBER SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
3. ALL HARDWARE ITEMS SUPPLIED ARE AMERICAN STANDARD SIZES AND SHALL BE GALVANIZED AND CONFORM TO ASTM A153 OR ASTM B695.
4. FASTENERS, EXCEPT FOR SPECIAL BOLT AND COUPLINGS, ARE INSTALLED WITH LOCKWASHERS, AND DO NOT HAVE SPECIFIC TORQUE REQUIREMENTS. FASTENERS SHALL BE SECURED AS TIGHT AS POSSIBLE WITH CONVENTIONAL WRENCHES, UNLESS NOTED OTHERWISE.
5. SQUARE UP AND LEVEL INDIVIDUAL COMPONENTS, PARTICULARLY ANCHORS TO MINIMIZE THE NEED FOR SHIMMING BETWEEN THE COUPLINGS AND ANCHORS.
6. NO MORE THAN TWO SHIMS SHALL BE PLACED UNDER ANY ONE COUPLING. NO MORE THAN THREE SHIMS UNDERNEATH ANY PAIR OF COUPLINGS.
7. THE CONTRACTOR SHALL FURNISH TWO (2) COMPLETE SETS OF SHOP PLANS FOR APPROVAL BY THE ENGINEER A MINIMUM OF TWO WEEKS PRIOR TO INSTALLATION.
8. THE HINGE SHOULD BE AT LEAST 7'-0" ABOVE THE GROUND.
9. A SINGLE POST IF 7'-0" OR MORE FROM ANOTHER POST, SHALL HAVE A WEIGHT LESS THAN 45 LB./FT. TOTAL WEIGHT BELOW THE HINGE, BUT ABOVE THE SHEAR PLATE OF THE BREAKAWAY BASE, SHOULD NOT EXCEED 600 LB.
10. FOR TWO POSTS SPACED LESS THAN 7'-0" APART, EACH POST SHOULD HAVE A WEIGHT LESS THAN 18 LB./FT.
11. COUPLINGS SHALL NOT BE USED IN SIGN STRUCTURES WITH THREE SUPPORTS OR MORE IF POSTS ARE CLOSER THAN 7'-0" APART.
12. REFER TO DETAIL SHEET "FOOTING DETAILS FOR TYPE C BEAM" FOR FOOTER DETAILS.



~ ELEVATION VIEW ~

BRACKET SELECTION TABLE

I-BEAM POST SIZE	BRACKET NO. 1		BRACKET NO. 2		BRACKET NO. 3	
	MIN. "L"	MAX. "L"	MIN. "L"	MAX. "L"	MIN. "L"	MAX. "L"
6"	12'-0"	29'-0"	9'-0"	12'-0"	0	9'-0"
8"	14'-0"	29'-0"	10'-0"	14'-0"	0	10'-0"
10"	16'-0"	29'-0"	11'-0"	16'-0"	0	11'-0"
12"	18'-0"	29'-0"	13'-0"	18'-0"	0	13'-0"
14"	19'-0"	29'-0"	14'-0"	19'-0"	0	14'-0"
16"	21'-0"	29'-0"	15'-0"	21'-0"	0	15'-0"
18"	23'-0"	29'-0"	16'-0"	23'-0"	0	16'-0"
21"	25'-0"	29'-0"	18'-0"	25'-0"	0	18'-0"



~ PICTORIAL VIEW ~

RGX 060

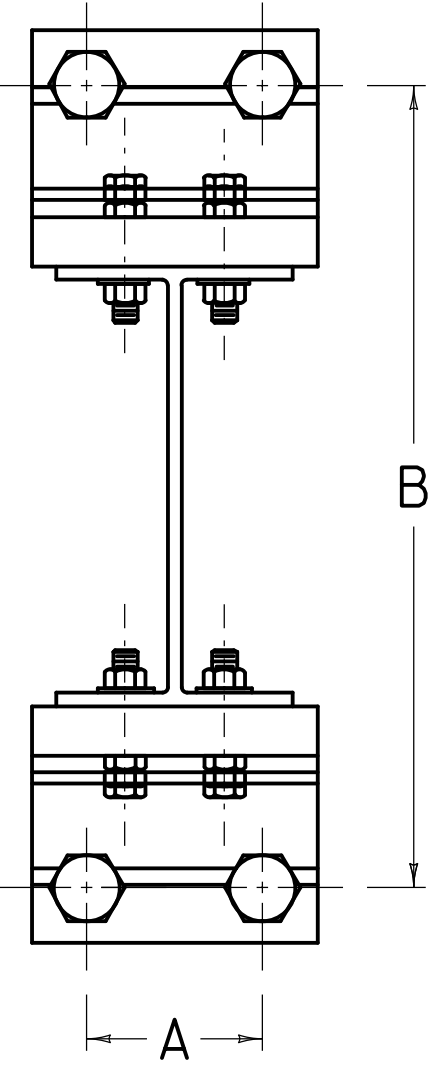
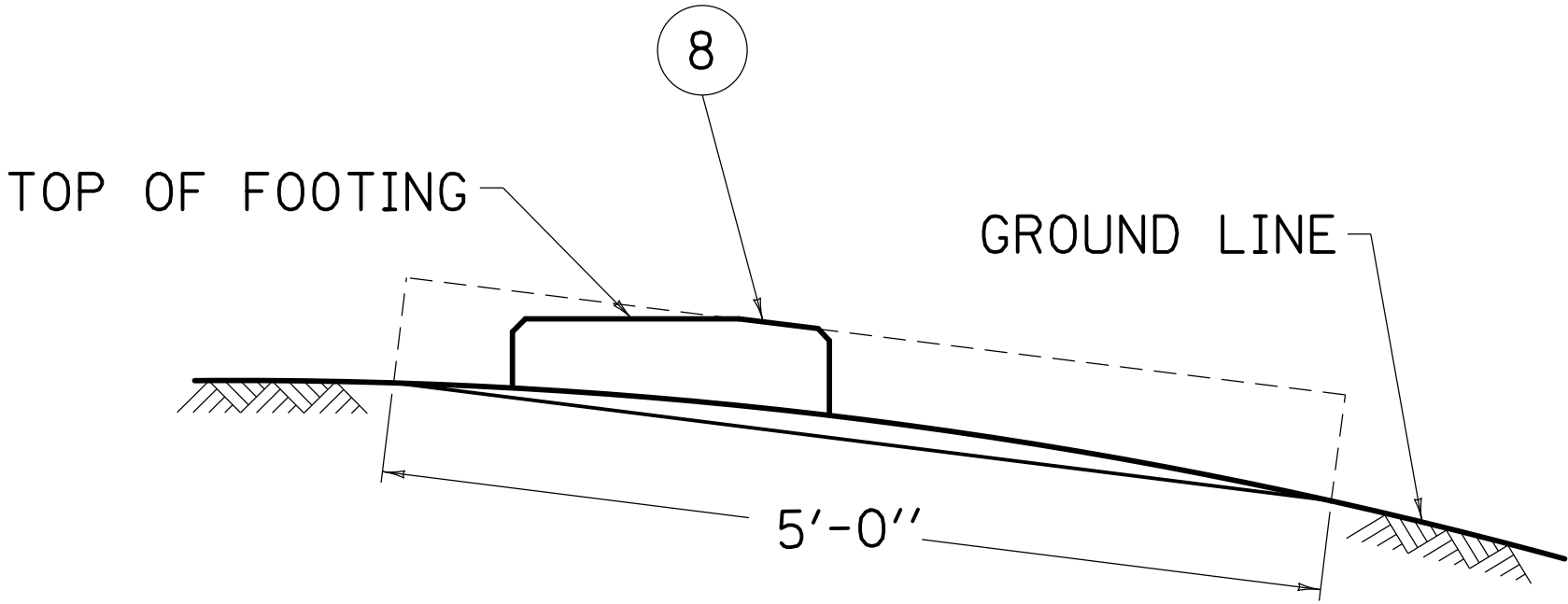
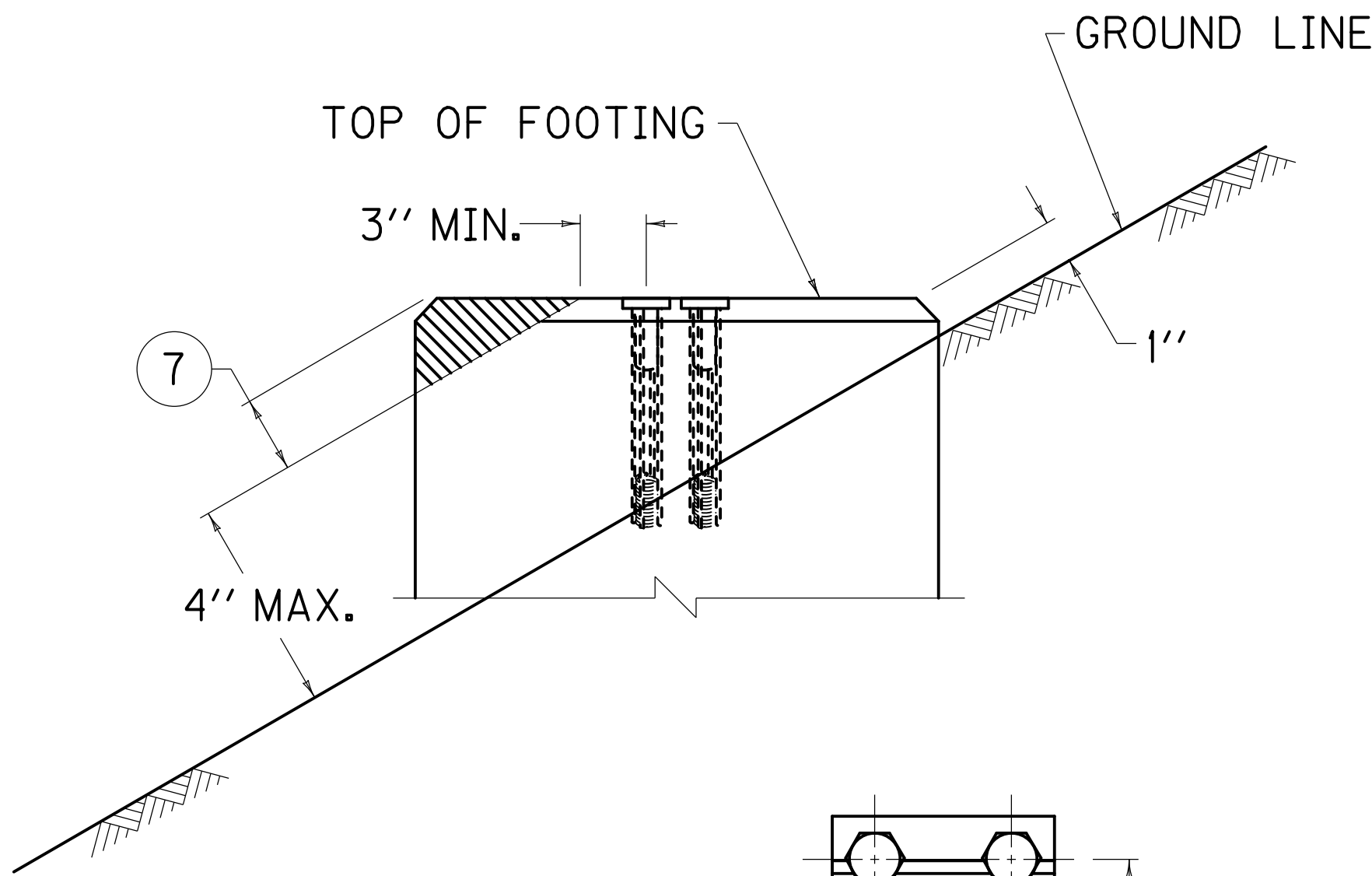
Breakaway Sign Support System
For Type C Beam

FOOTING SELECTION TABLE

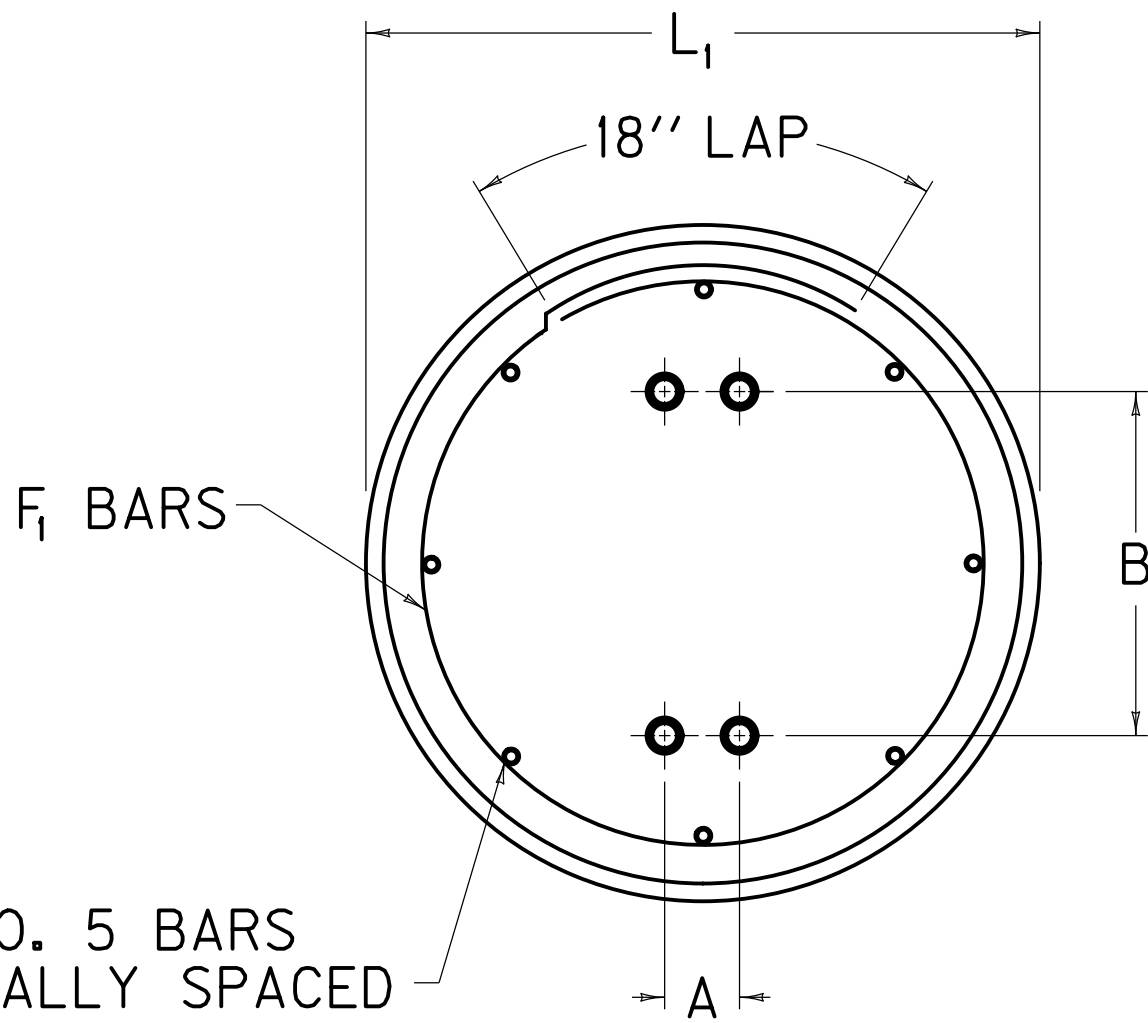
POST SIZE	L ₁ DIA.	D ₁ DEPTH	STEEL F ₁ BARS		REINF.	CONC.
			QTY	SIZE	LBS.	CU. YD.
W6	2'-0"	5'-0"	5	#4	57	0.58
W8	2'-6"	7'-0"	7	#4	88	1.27
W10	3'-0"	8'-0"	8	#4	110	2.09
W12	3'-0"	8'-0"	8	#4	110	2.09
W14	3'-0"	9'-0"	9	#4	124	2.36
W16	3'-6"	9'-0"	9	#4	133	3.21
W18	3'-6"	9'-0"	9	#4	133	3.21
W21	4'-0"	9'-0"	9	#4	143	4.19

~NOTES~

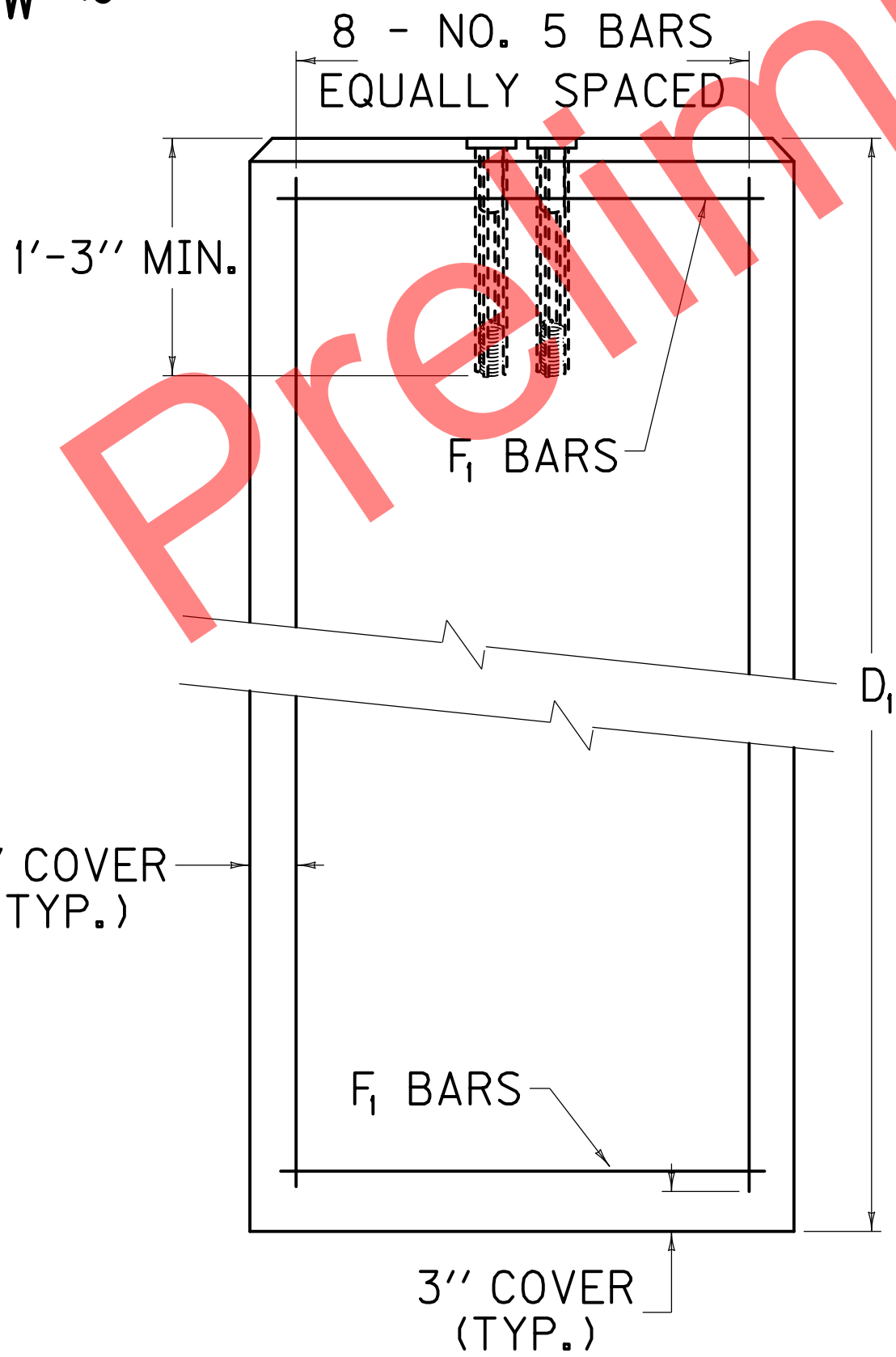
- ENTER FOOTING SELECTION TABLE WITH REQUIRED POST SIZE AND FIND REQUIRED FOOTING VALUES AS SHOWN IN DETAILS.
- THE ANCHOR SHALL BE 304 STAINLESS STEEL WITH 1053 STEEL ROD AND COIL.
- FORM TOP 1'-0" OF THE FOOTING.
- USE CLASS "A" CONCRETE IN ALL FOOTINGS.
- ACTUAL DIMENSIONS "A" & "B" SHOULD BE OBTAINED FROM THE MANUFACTURER OR MEASURED FROM THE ASSEMBLED BRACKETS PRIOR TO PLACEMENT OF ANCHORS.
- TO INSURE PROPER SPACING AND ALIGNMENT OF ANCHORS, IT IS RECOMMENDED THAT ALL ANCHORS BE HELD IN PLACE BY A RIGID TEMPLATE WHILE THE CONCRETE IS PLACED AND CURED.
- FOOTING PROJECTIONS ABOVE GROUND LINE SHALL BE MINIMIZED. THE MAXIMUM PERMISSIBLE FOOTING PROJECTION SHALL BE 4" ON THE LOWER SLOPE SIDE. WHERE NECESSARY, THE SHADED AREA OF THE FOOTING SHALL BE REMOVED AND REINFORCEMENT SHALL BE BENT TO FIT.
- THE TOP OF THE FOOTING SHALL NOT PROJECT MORE THAN 4" ABOVE ANY 5'-0" CHORD ALIGNED PERPENDICULAR TO THE EDGE OF THE ROADWAY BETWEEN A POINT ON THE GROUND SURFACE ON ONE SIDE OF THE SUPPORT TO A POINT ON THE GROUND SURFACE ON THE OTHER SIDE OF THE SUPPORT.



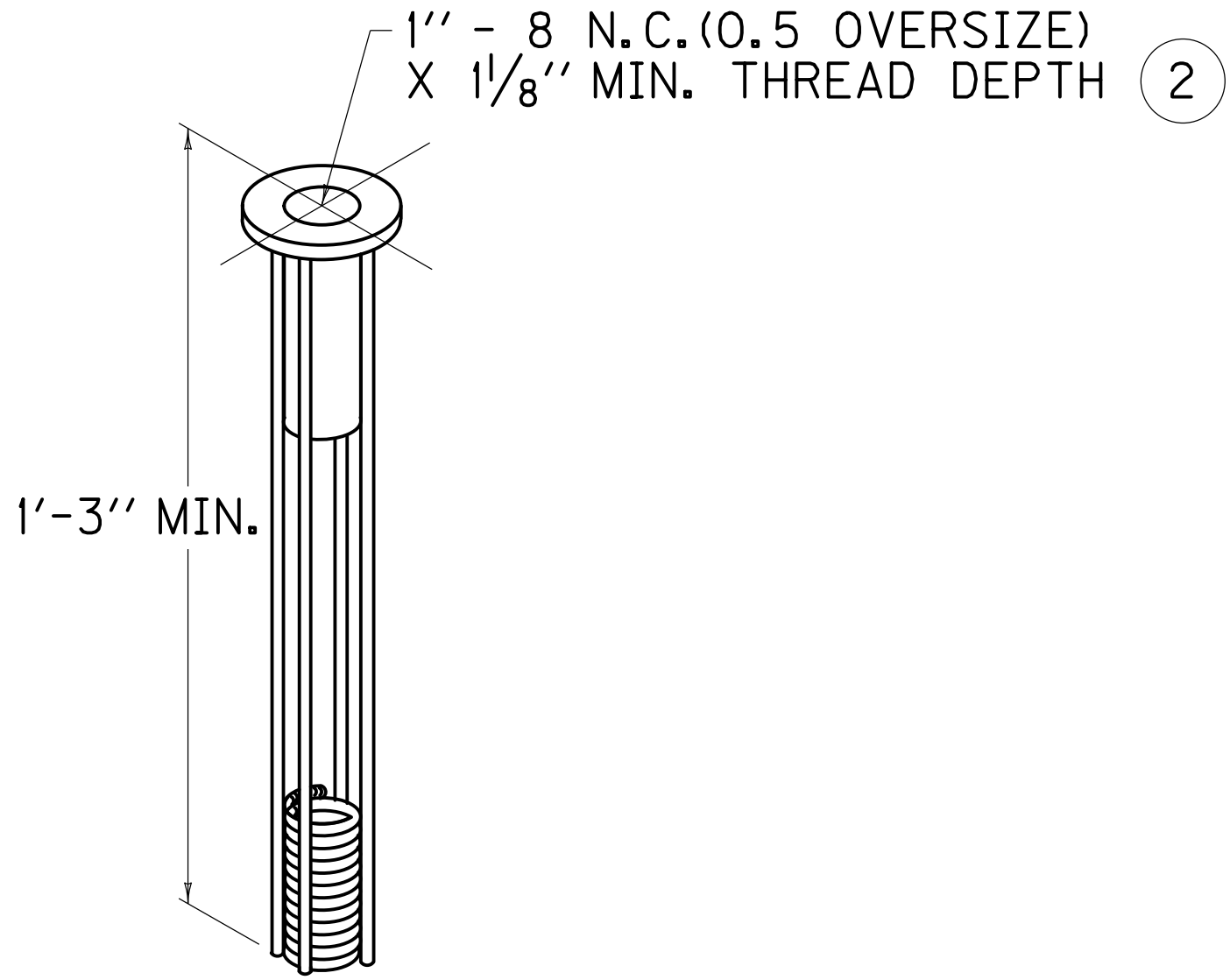
~ PLAN VIEW ~



~ TOP VIEW ~



~ SIDE VIEW ~



~ ANCHOR PICTORIAL VIEW ~

RGX 061

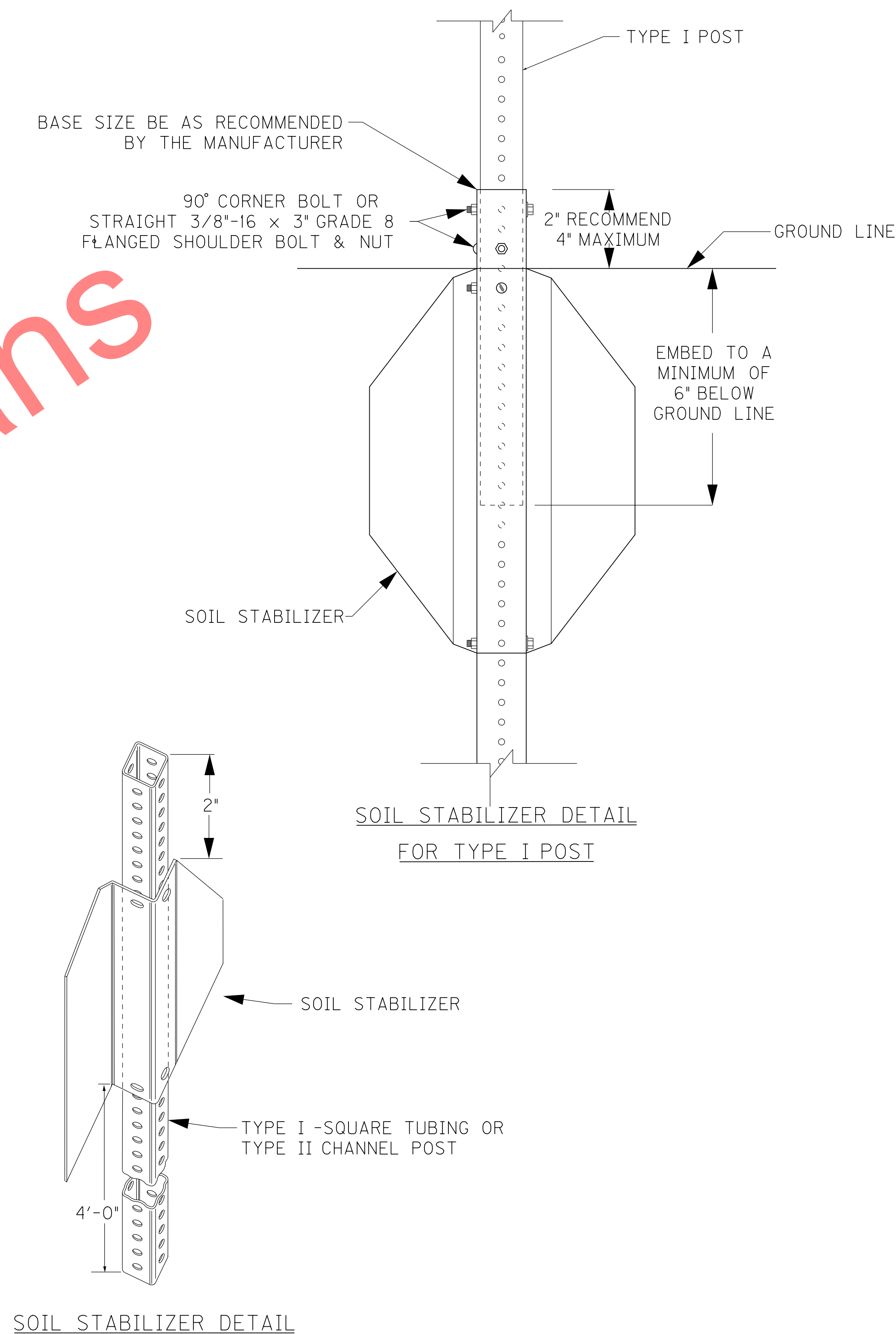
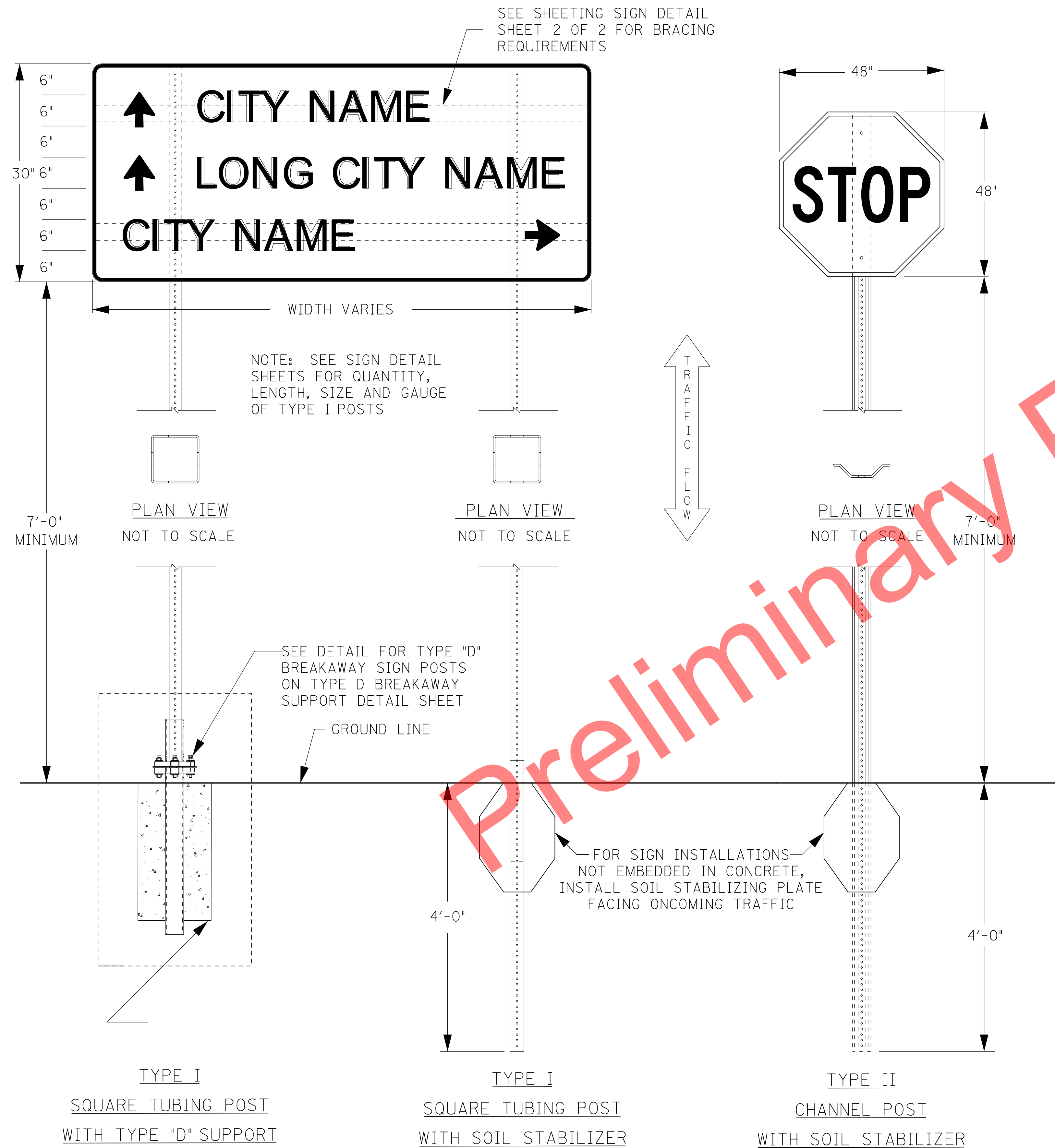
Footing Details for Type C Beam

FILE NAME: P:\CIVIL\1471\SIGNING PLANS\T00800SN.DGN

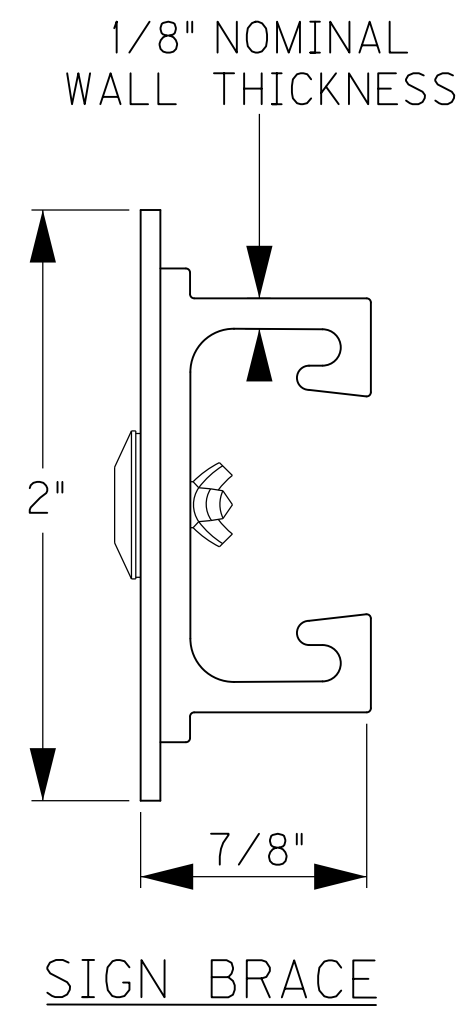
USER: b'reague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8.11.7.443

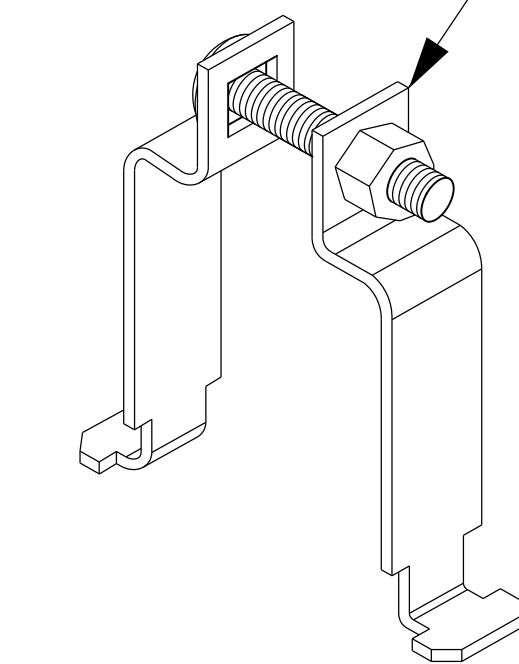


NOT TO SCALE

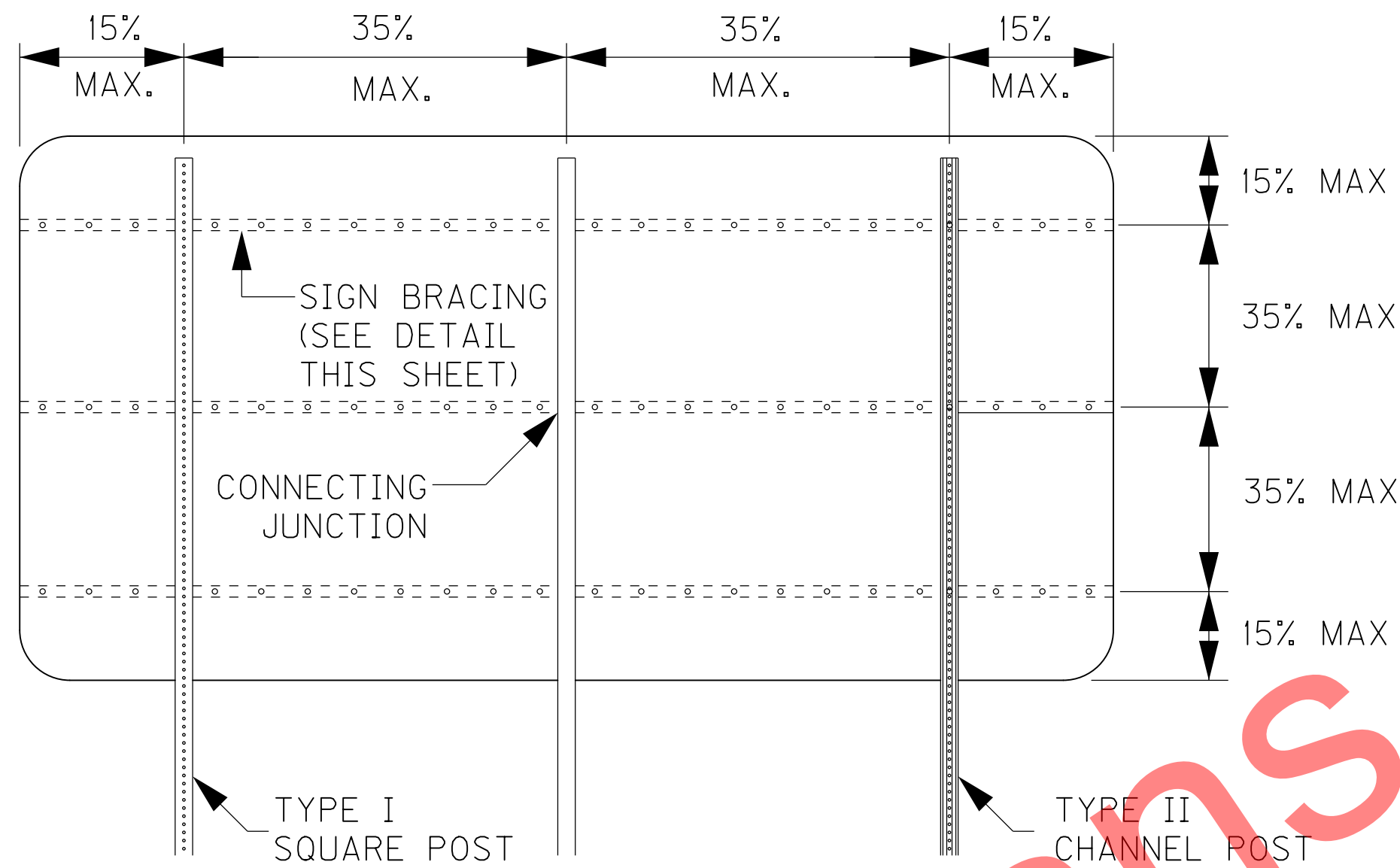


SQUARE POST CLAMP:
11 GAUGE, TYPE 304, #2B FINISHED STAINLESS STEEL
WITH STAINLESS STEEL CARRIAGE BOLT

CLAMPS CAN BE TWIST LOCKED INTO PLACE
WITHOUT SLIDING THE CLAMPS FROM AN OPEN
END OF THE CHANNEL BRACE

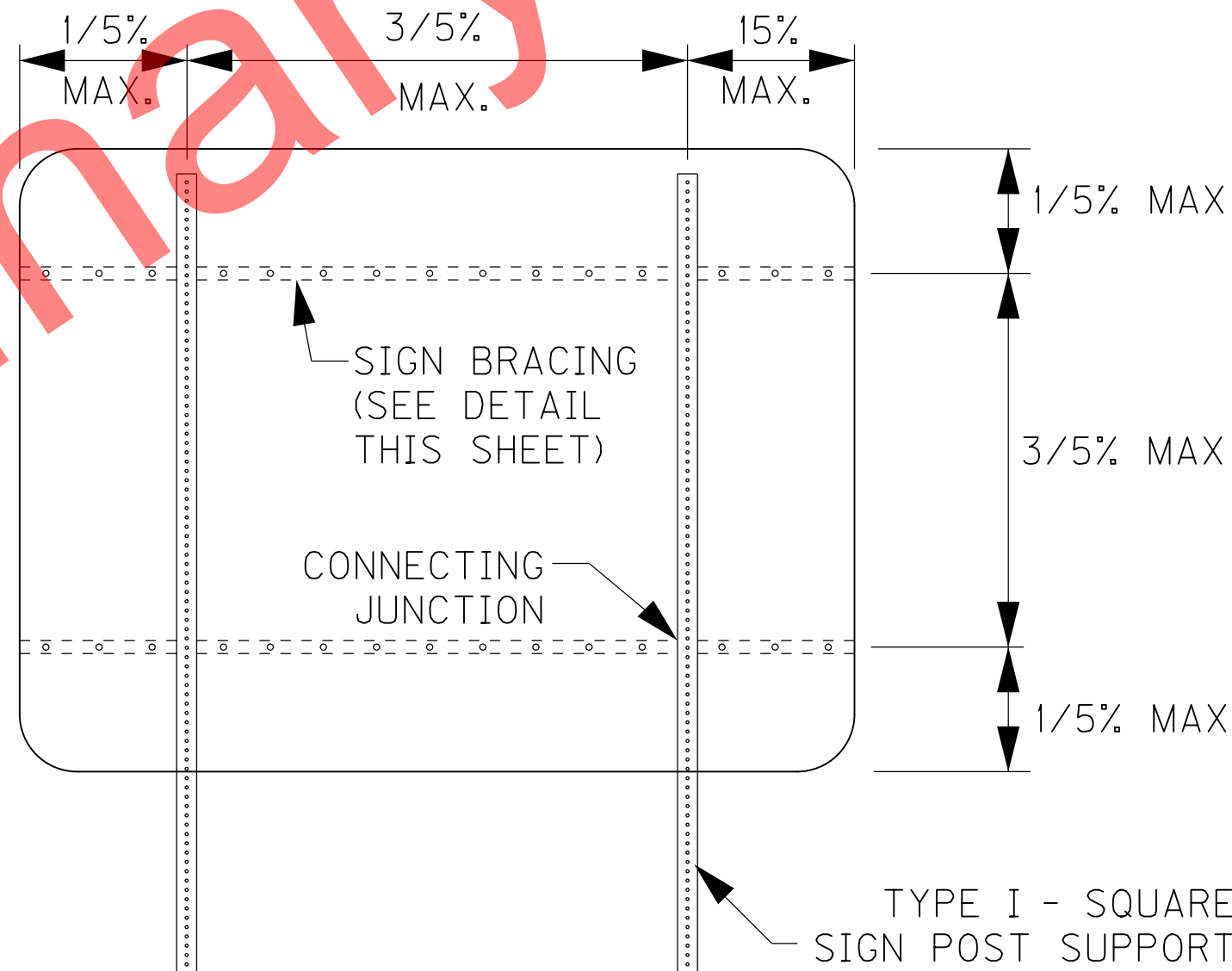


SQUARE POST CLAMP

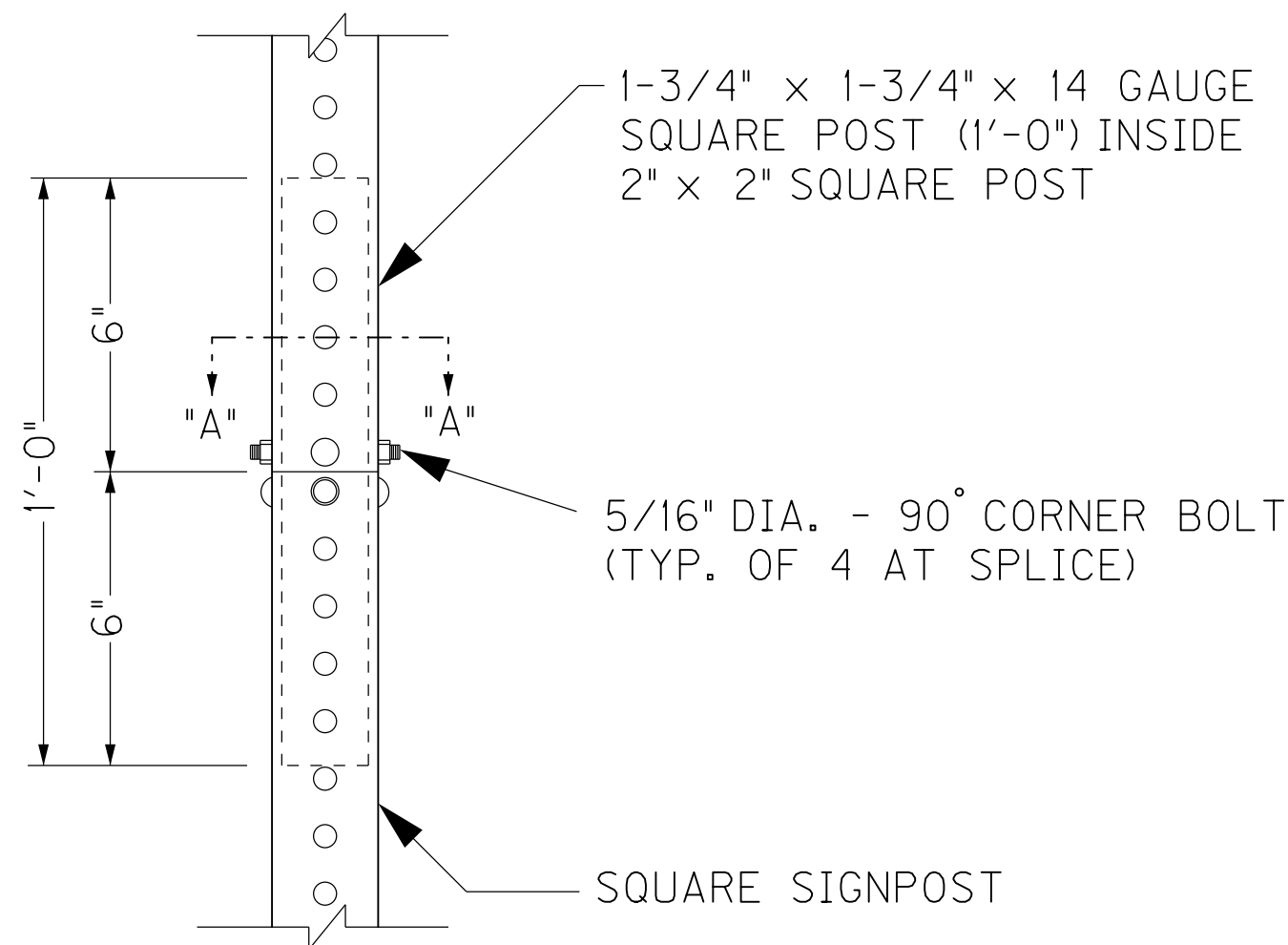


3 POST - BRACING DIAGRAM

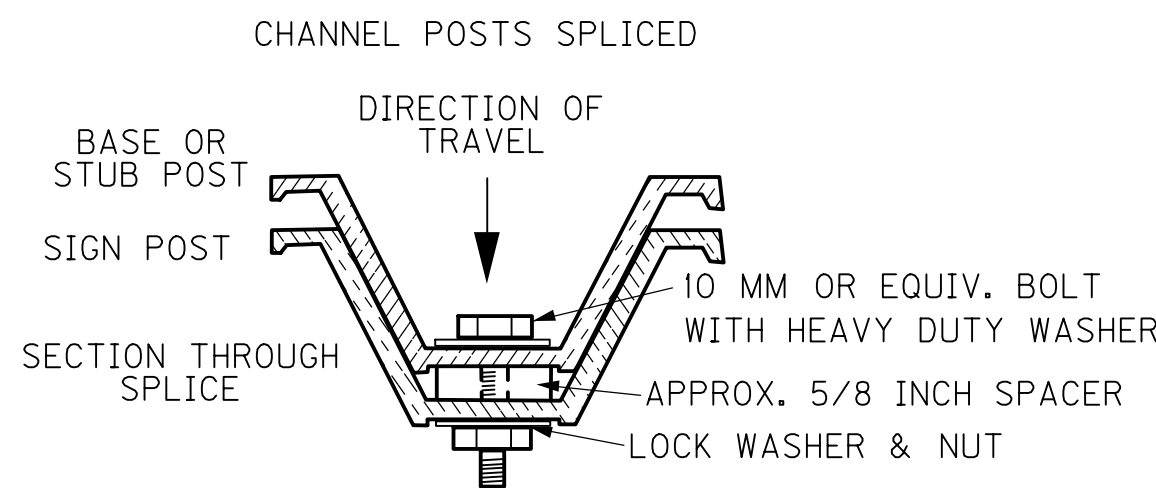
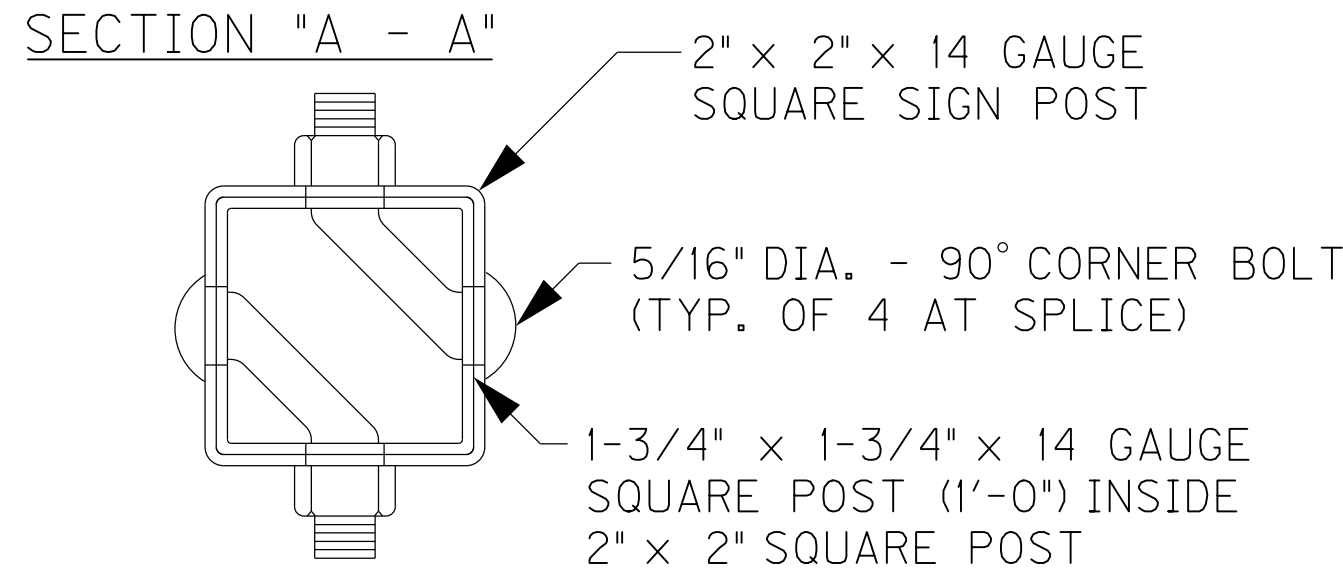
- NOTE:
1. MAXIMUM AREA PER CONNECTING JUNCTION = 16 SQ. FT.
 2. BRACING SHOULD NOT BE SPLICED WITHIN 6" OF A BRACE TO POST JUNCTION.



2 POST - BRACING DIAGRAM

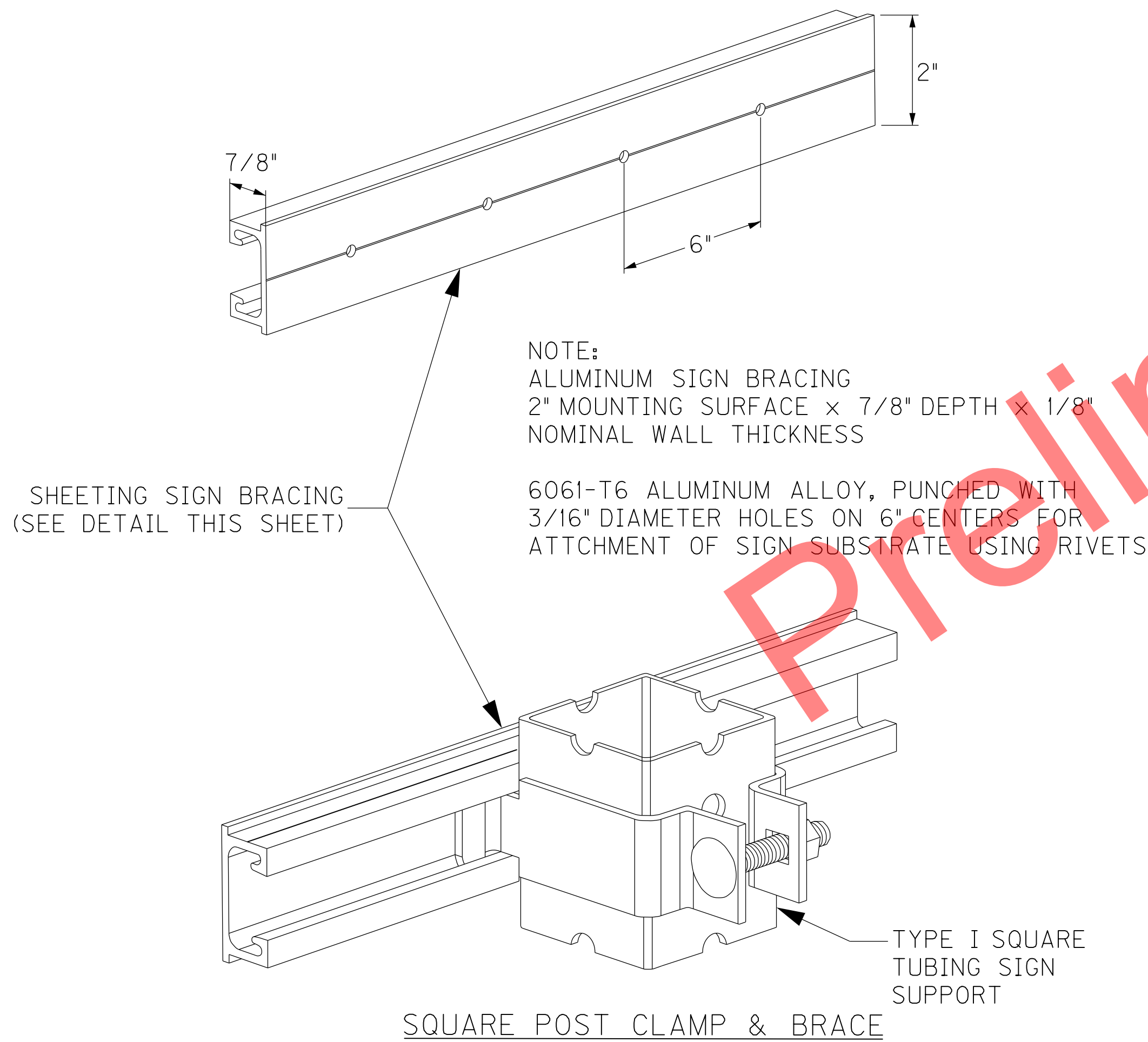


SQUARE SIGNPOST
SPLICE DETAIL



GENERAL INSTALLATION INSTRUCTIONS OF U CHANNEL POSTS
WITH BREAKAWAY FEATURE

THE BASE OR STUB POST SHOULD BE IN FRONT FACING TRAFFIC.
THE SIGN AND THE STUB POSTS SHOULD HAVE AN 8 INCH OVERLAP.
DETERMINE THE PROPER SIZE SIGN POST AS PER THE SIGNING PLANS.
MATCH THE SIGN POST TO THE STUB POST AS IN THE TYPE AND WEIGHT OF THE POST (I.E. 2 LB, 3 LB, 4 LB, ETC.)
DRIVE THE STUB INTO THE GROUND UTILIZING A DRIVE CAP. THE STUB HEIGHT IS TO BE 4 INCH MAXIMUM FROM EXISTING GROUND.
REAM OUT THE SPLICE HOLES UNTIL A 10 MM OR EQUIV. BOLT CAN BE INSERTED THROUGH THE POSTS. THE TOP BOLT SHOULD BE 1 INCH MINIMUM FROM THE TOP OF STUB.
COAT THE SPLICE HOLES AND THE BOLTS WITH ZINC RICH PAINT PASTE. ASSEMBLE THE SIGN POST TO THE STUB WITH THE PROPER HARDWARE AS PER THE CURRENT SPECIFICATIONS.
THE TURN OF NUT METHOD SHALL BE UTILIZED IN TIGHTENING THE BOLTS AND NUTS.
(THE NUT IS BROUGHT TO A SNUB TIGHT CONDITION TO INSURE THAT ALL PARTS ARE BROUGHT TOGETHER INTO FULL CONTACT WITH EACH OTHER. THEN THE NUT IS TIGHTENED A HALF AN ADDITIONAL TURN.)
ANY SINGLE U CHANNEL POSTS MAY BE DIRECT DRIVEN.



SQUARE POST CLAMP & BRACE

TYPE I SQUARE
TUBING SIGN
SUPPORT

FILE NAME: P:\CIVIL\1471\SIGNING\PLANS\T01005N.DGN

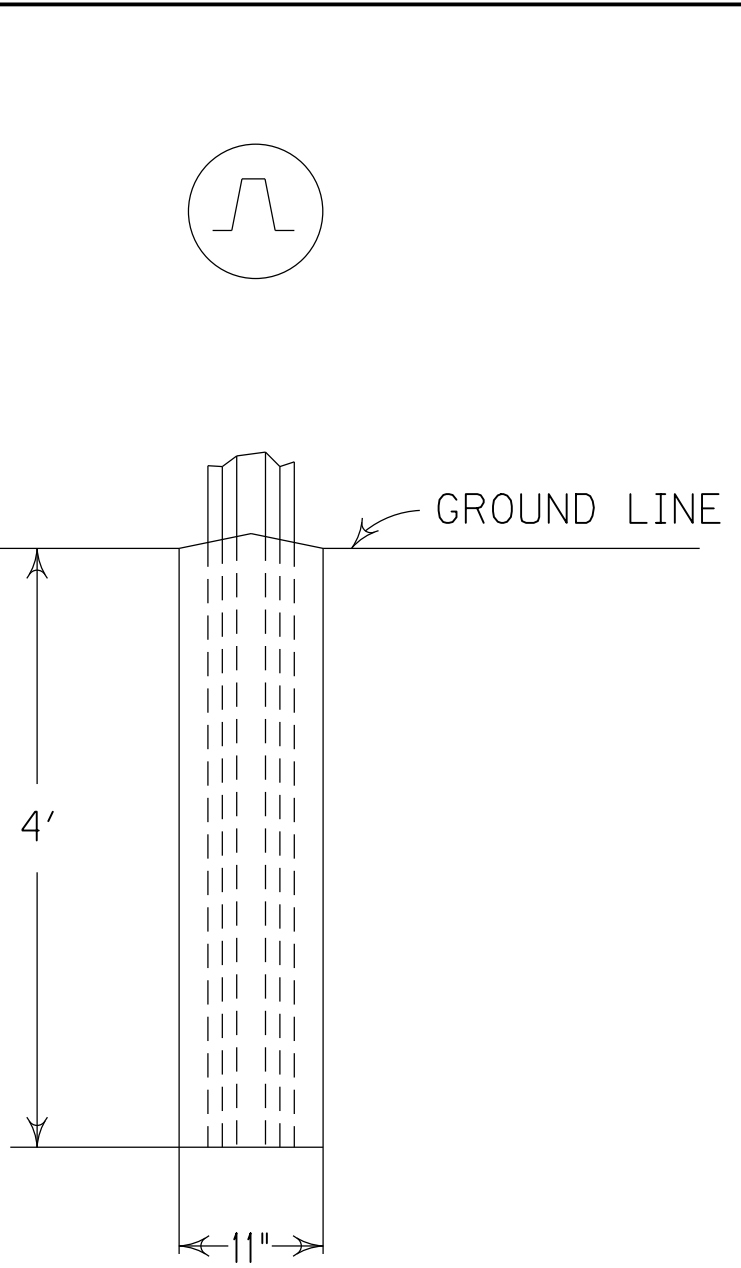
USER: b'reague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

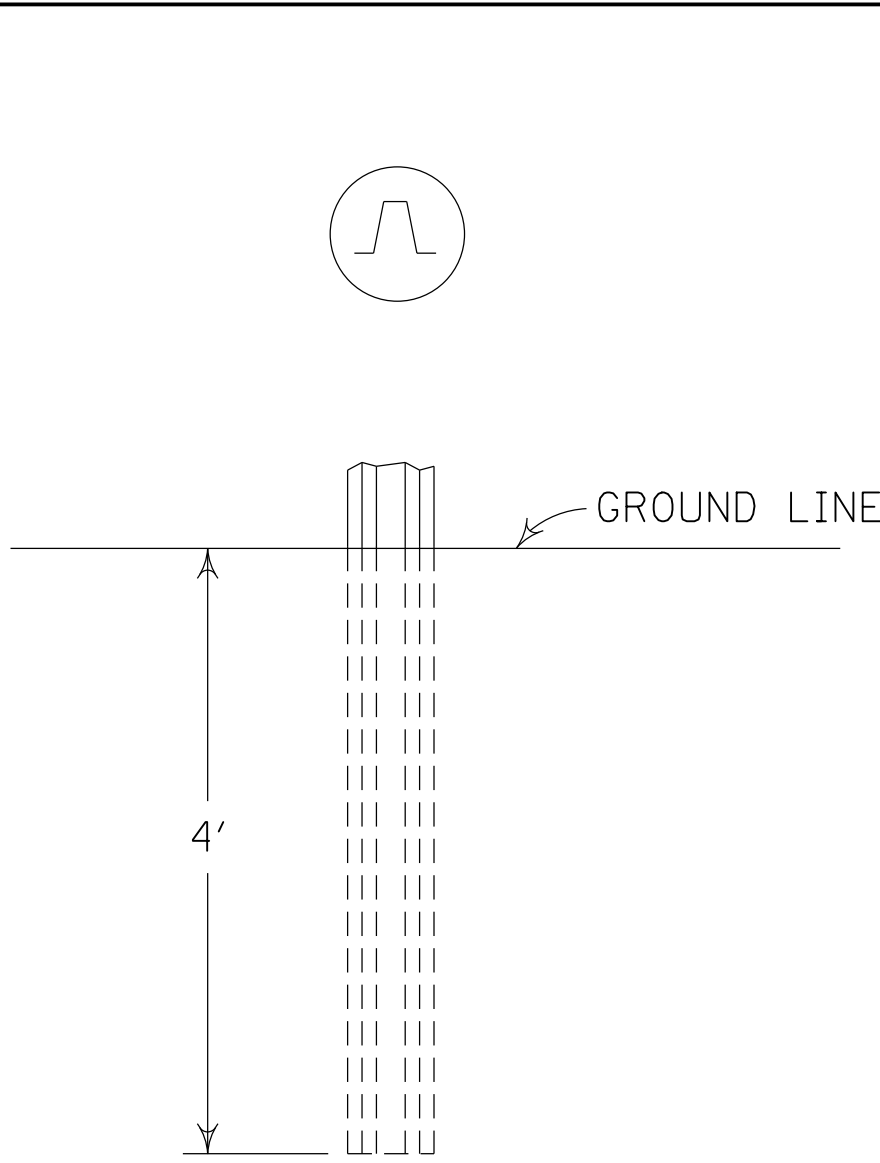
MicroStation v8.11.7.443

CHANNEL POST DETAILS

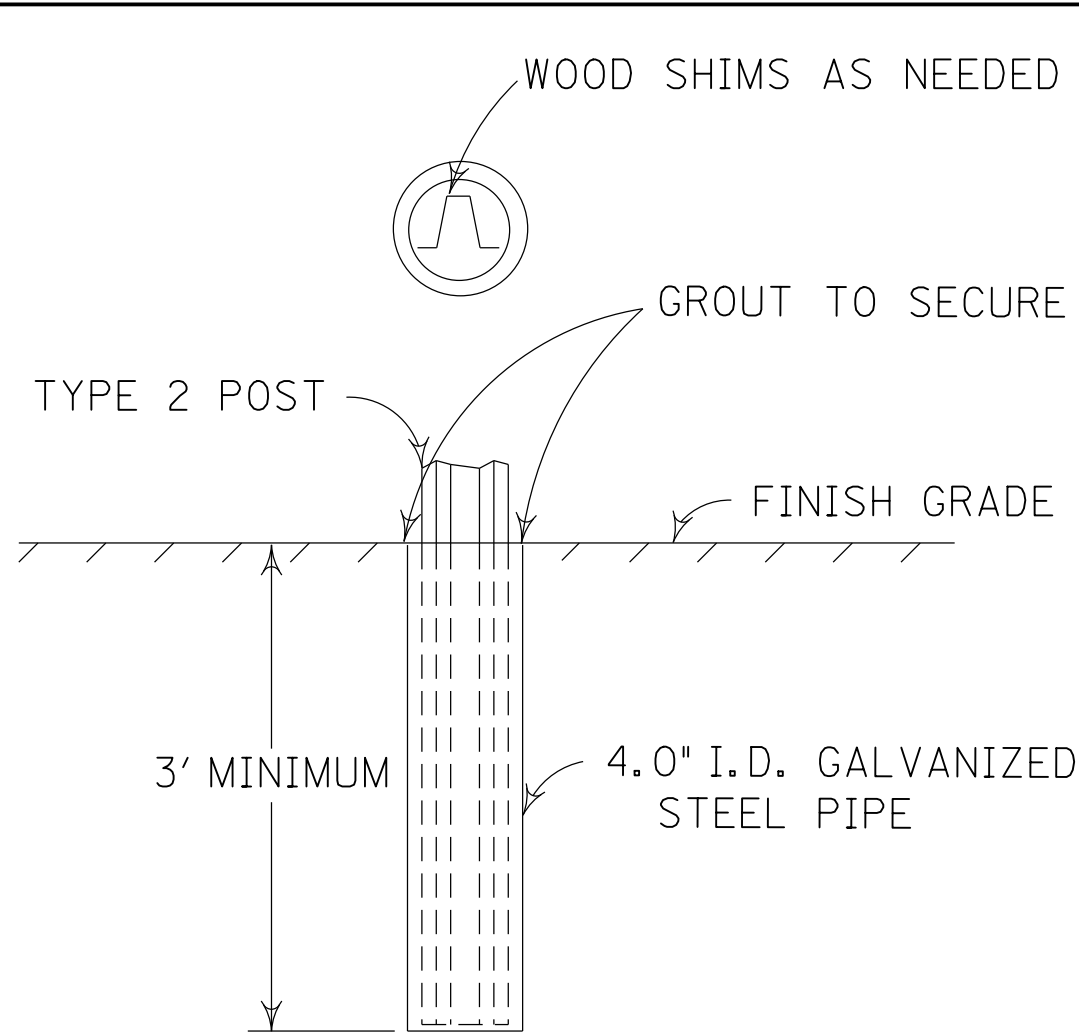
TYPE "B" CONCRETE BASE



TYPE "C" DRIVEN BASE



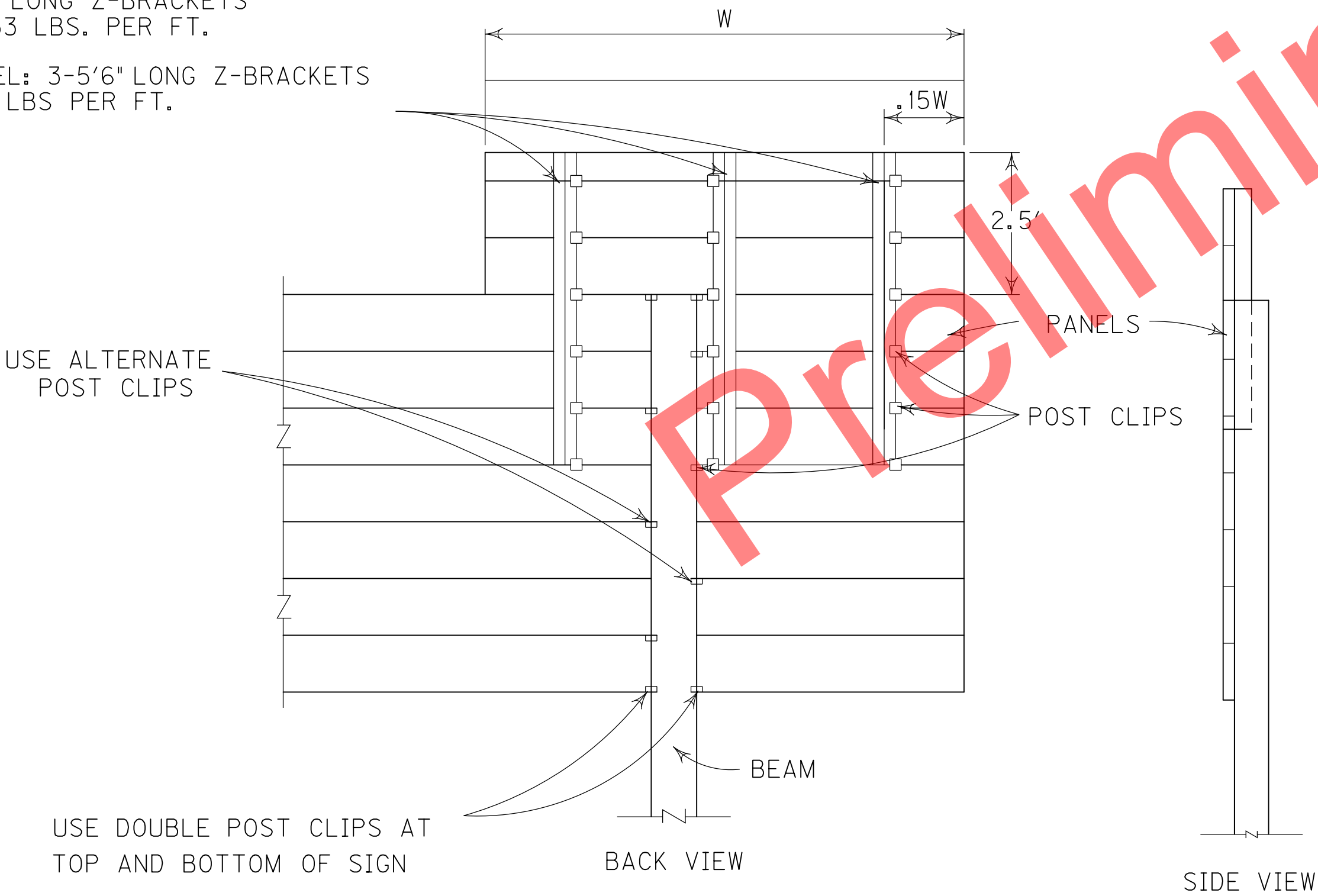
TYPE "D" PIPE BASE



THE COST FOR 4.0" I.D. GALVANIZED STEEL PIPE FOR TYPE "D" BASE AND THE WORK FOR THE INSTALLATION SHALL BE INCLUDED IN THE BID ITEM FOR STEEL POST TYPE 2.

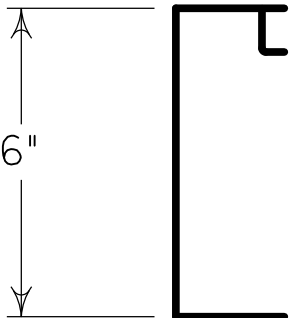
DETAIL FOR EXIT NUMBER SIGN ATTACHMENT AND TYPICAL POST CLIP ARRANGEMENT

ALUMINUM: 3-5'6" LONG Z-BRACKETS
3" X 2 1/16" @ 2.33 LBS. PER FT.
OR
GALVANIZED STEEL: 3-5'6" LONG Z-BRACKETS
3" X 2 3/4" @ 6.7 LBS PER FT.



NOTE: THE COST FOR ATTACHING EXIT NUMBER SIGN SHALL BE INCLUDED IN THE BID ITEM FOR SIGN BASE MATERIAL FOR PANEL SIGNS AND SHALL INCLUDE ALL Z-BRACKETS AND HARDWARE. THE EXIT NUMBER SIGN SHALL BE CENTERED OVER THE LEFT OR RIGHT SIDE OF SIGN AS SHOWN ON THE PLANS.

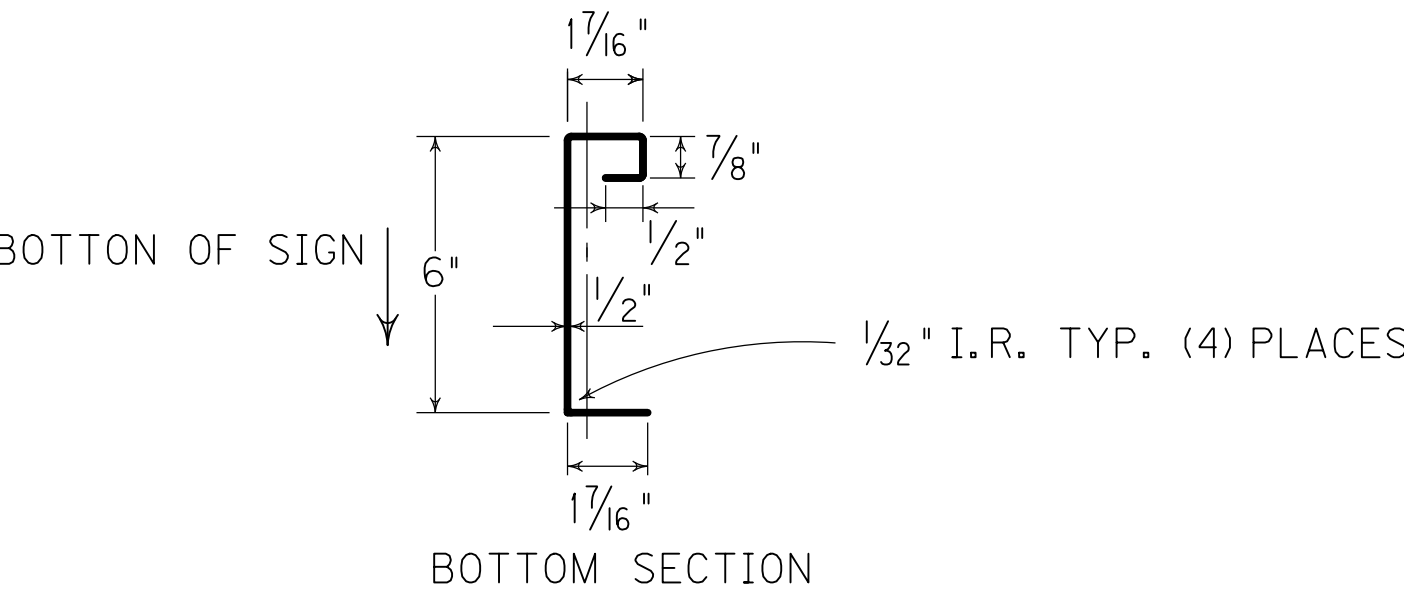
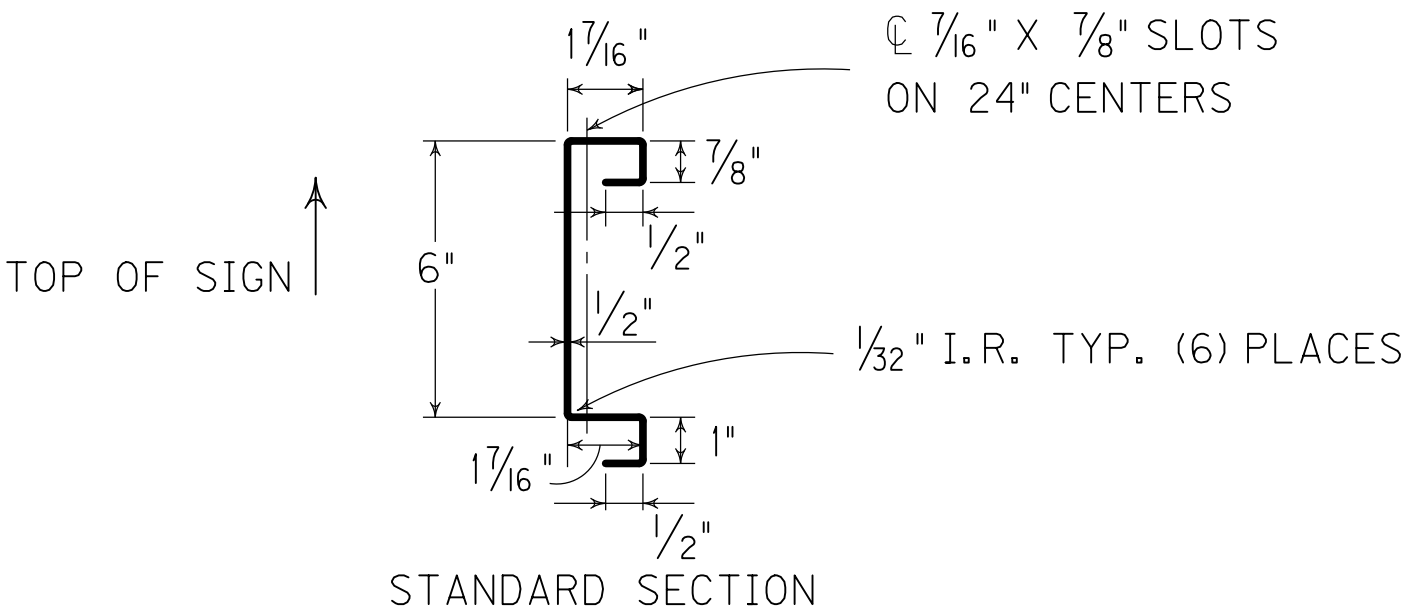
PANEL DETAILS



MINIMUM WEIGHT PER LINEAR FOOT
6" PANEL - 1.115 LBS.
12" PANEL - 2.485 LBS.

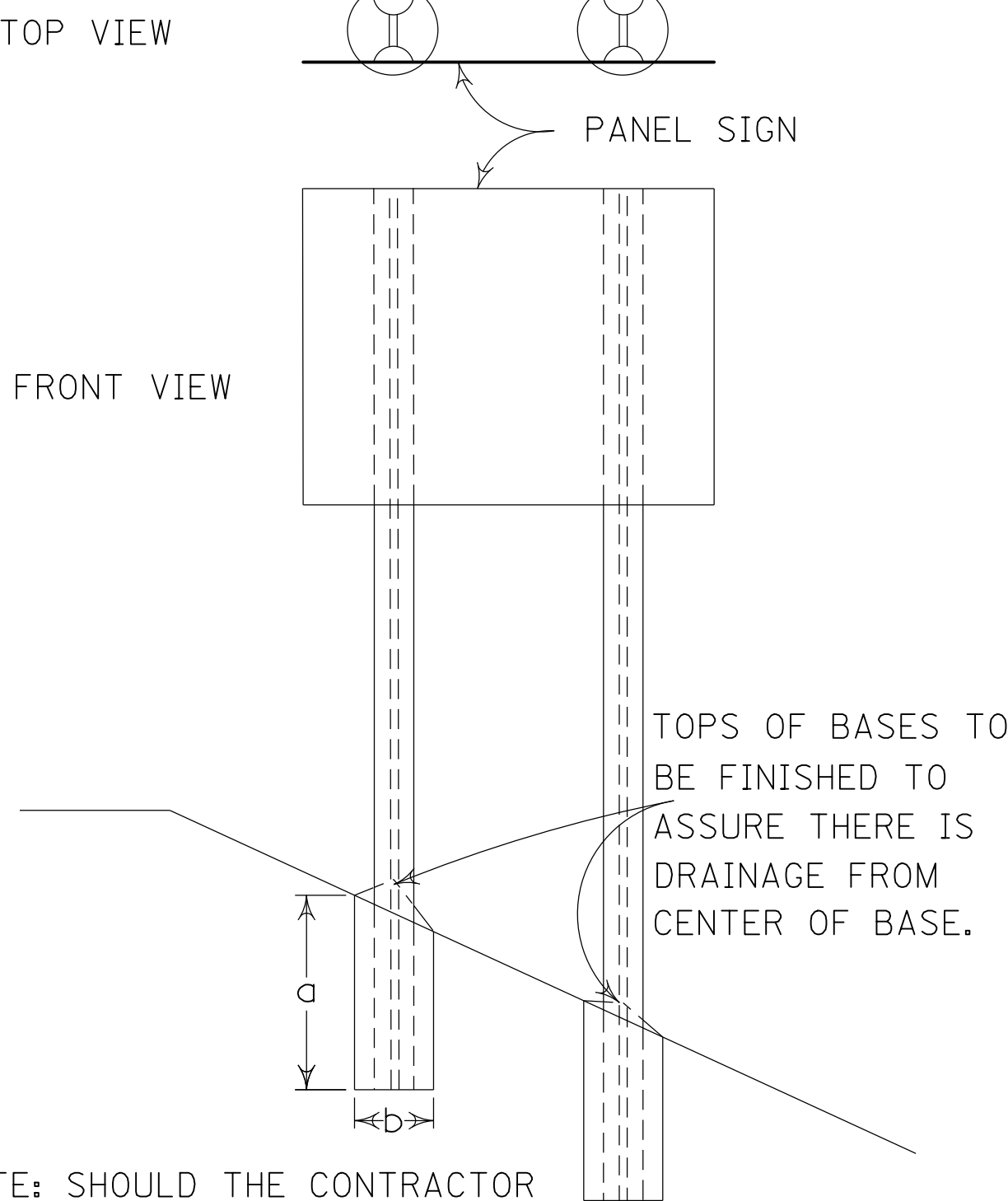


TYPICAL SECTIONS
ALUMINUM SIGN PANEL EXTRUSIONS

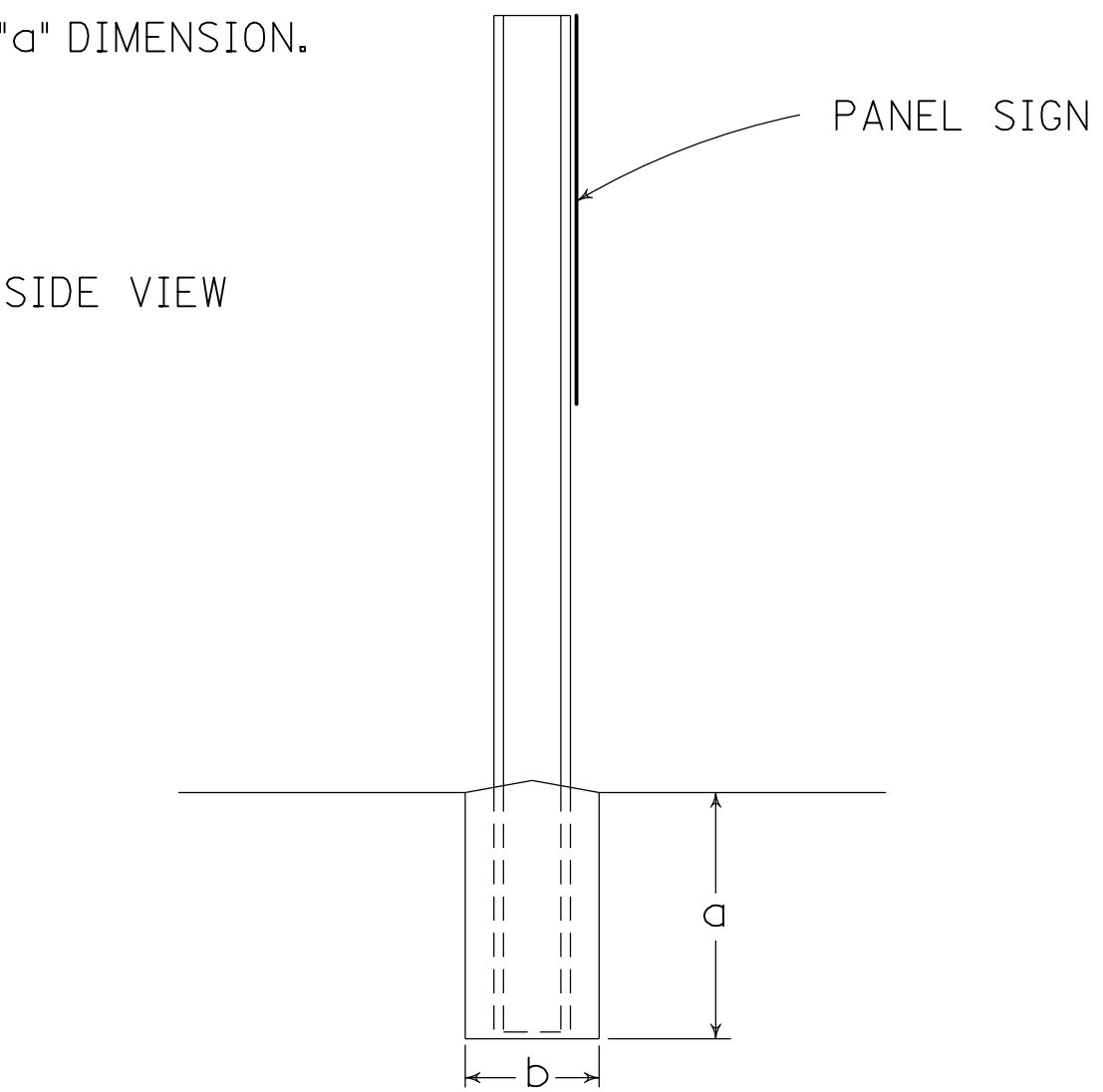


TYPICAL SECTIONS
STEEL SIGN PANEL EXTRUSIONS

TYPICAL BEAM WITH TYPE "A" CONCRETE BASE DETAIL

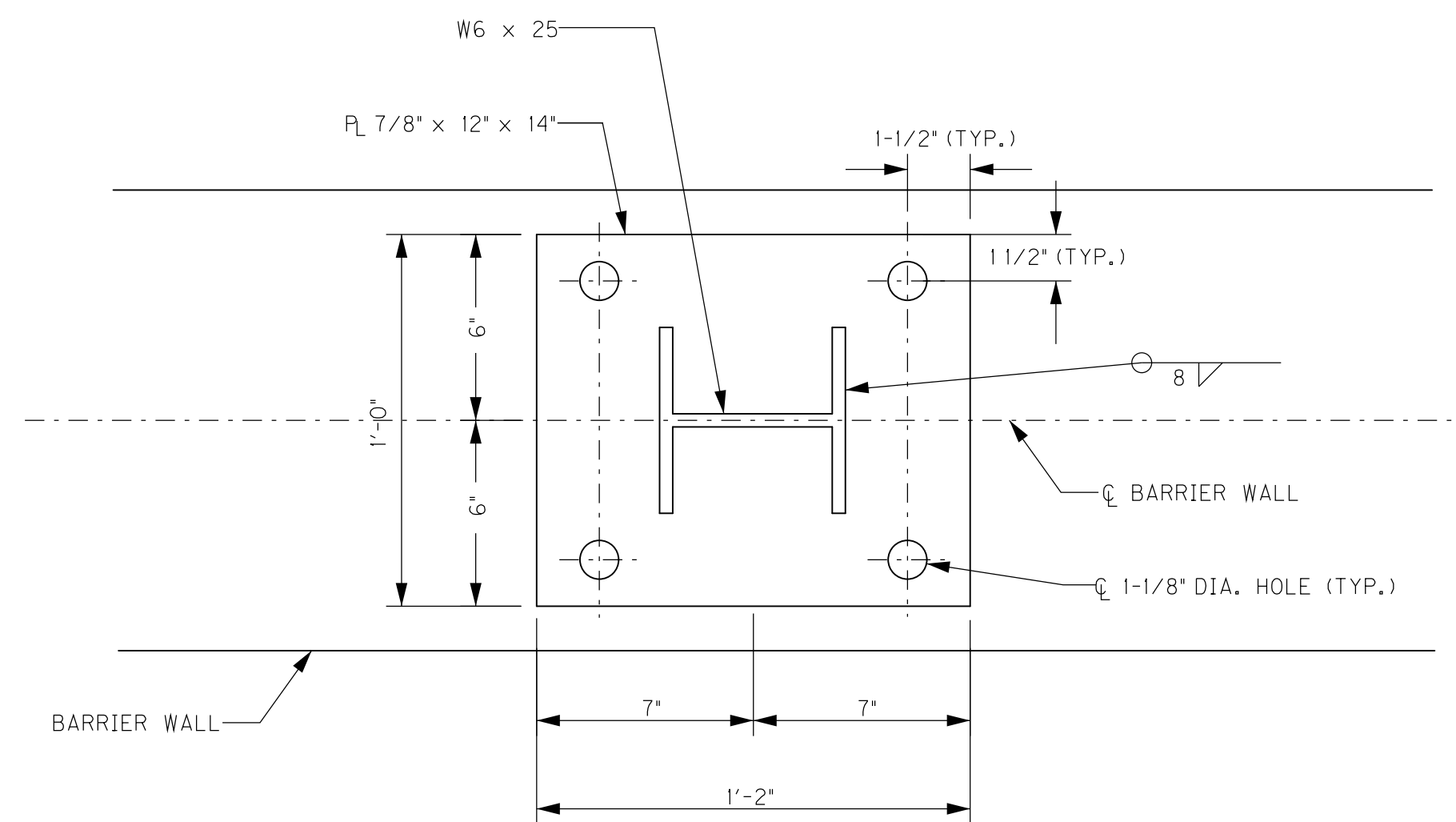


NOTE: SHOULD THE CONTRACTOR OVERDRILL THE HOLE, EXTRA CONCRETE WILL BE AT THE CONTRACTOR'S EXPENSE. PAYMENT WILL BE DETERMINED BY THE "a" DIMENSION.

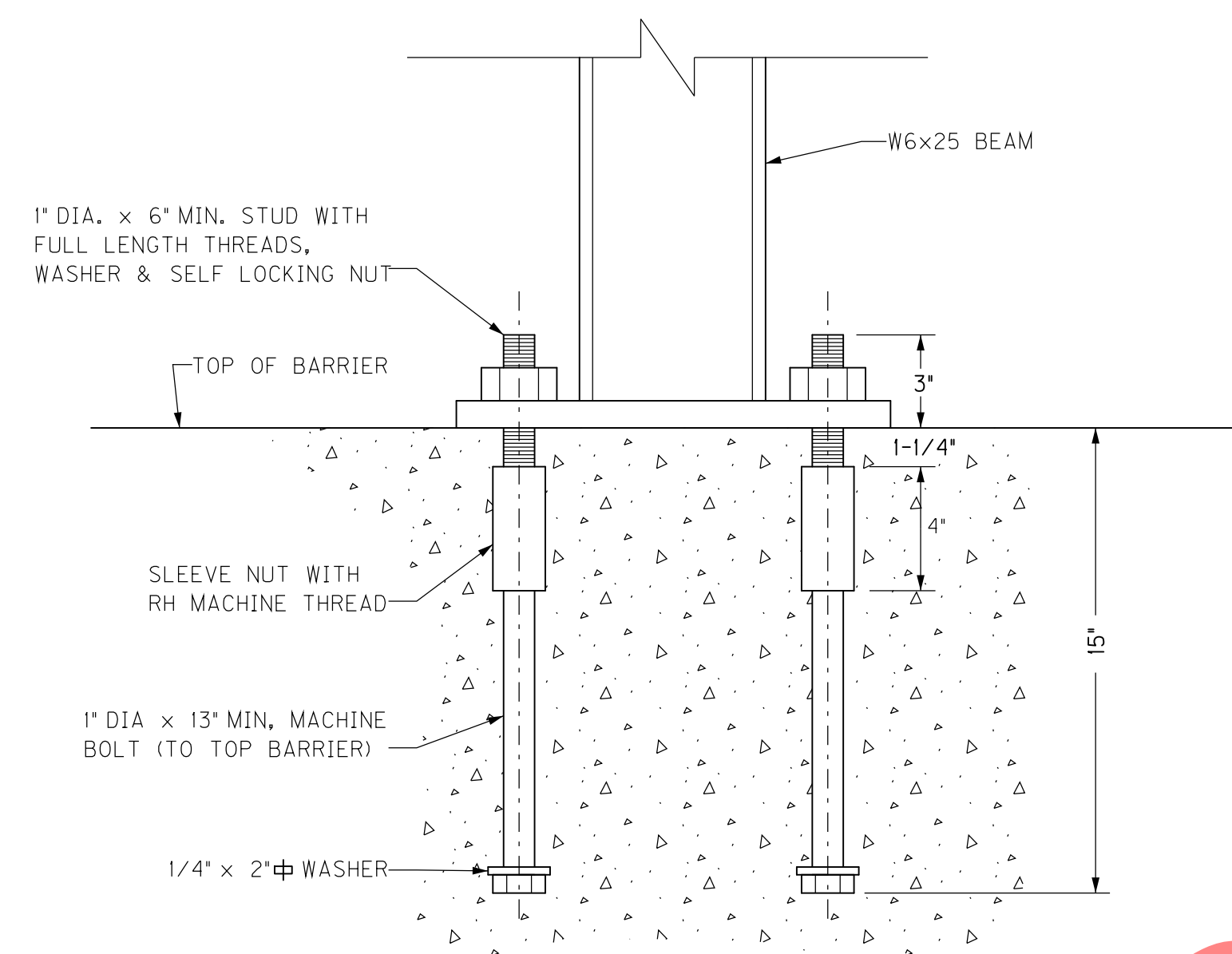


NOT TO SCALE

COUNTY OF	ITEM NO.	SHEET NO.
Campbell	6-2021.00	T12



PLAN VIEW DETAIL



ANCHOR DETAIL

GENERAL NOTES:

POST CLIPS, BRACKETS AND SIGN TO BE CAPABLE OF SUSTAINING 20 LBS. PER SQUARE FOOT OF SIGN AREA.

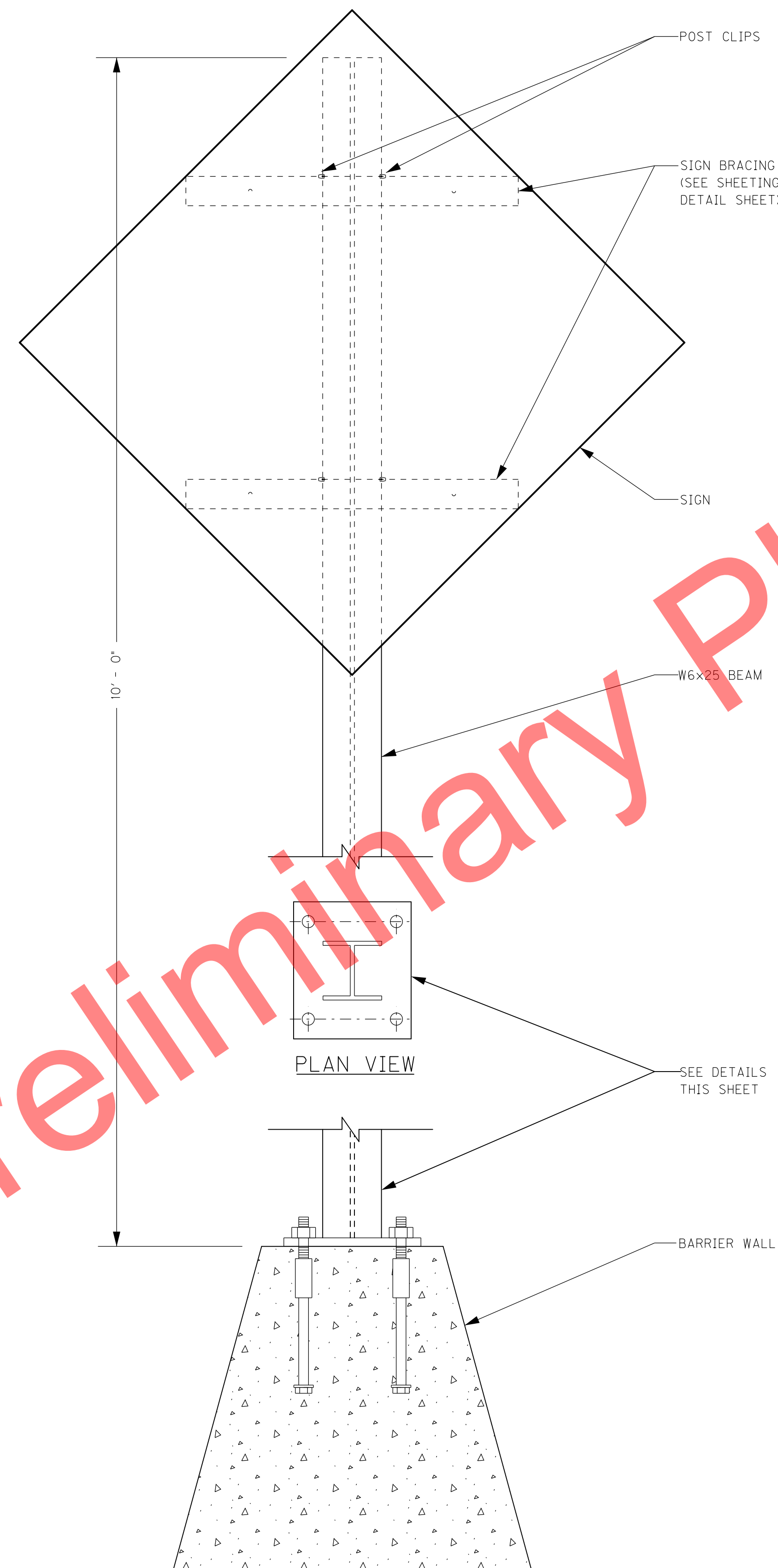
THE COST FOR MAKING WALL POST AND ANY HARDWARE REQUIRED TO COMPLETE THE INSTALLATION SHALL BE INCLUDED IN THE COST OF BARRIER WALL POST.

ANCHOR ALTERNATIVES:

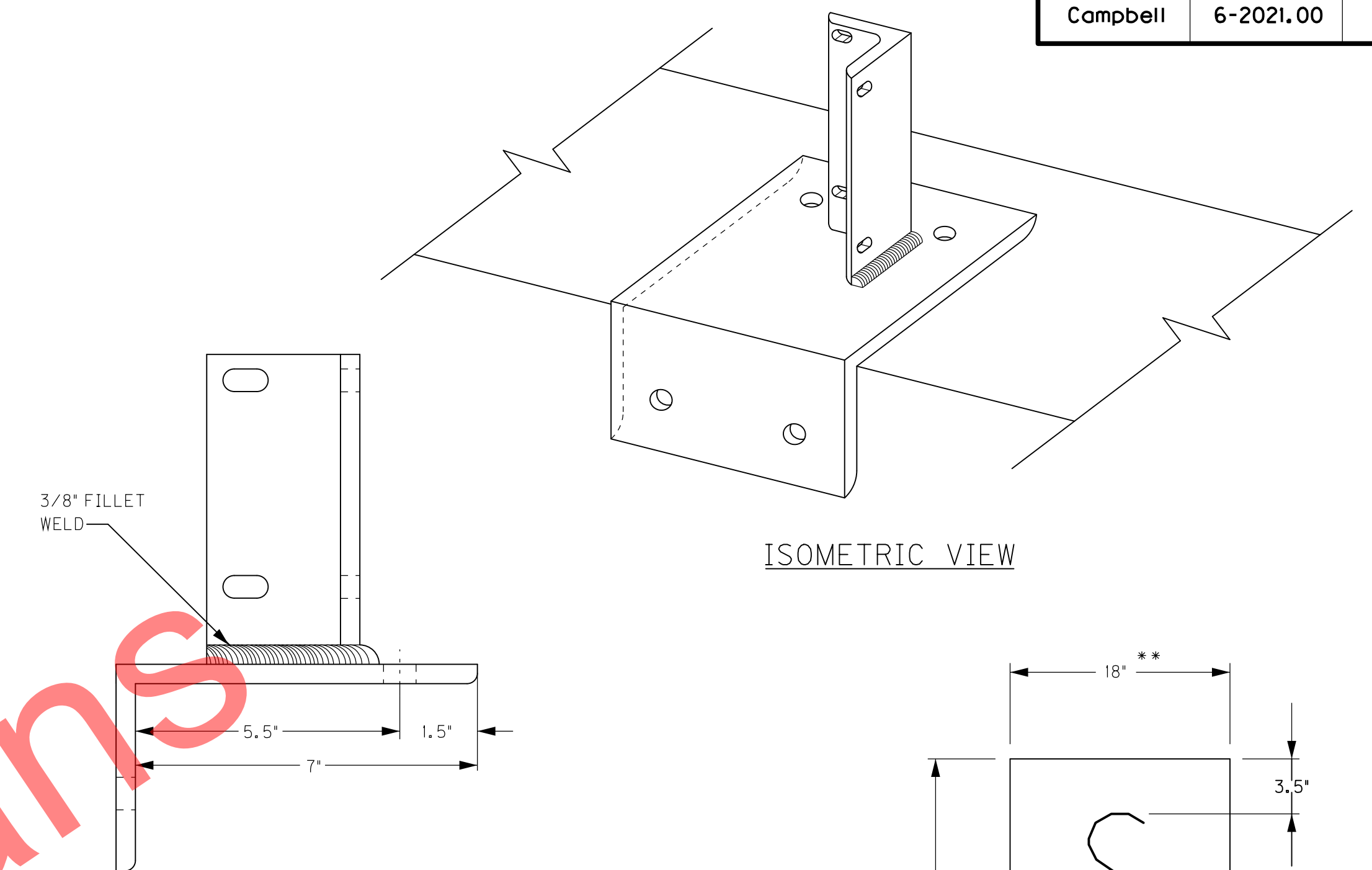
DRILL 1-3/8" O X 14" HOLE FOR #8 X 16" REINFORCING BAR WITH 1" X 4" MACHINE THREADS. BOND ANCHORS IN HOLE WITH POLYESTER RESIN.

MATERIAL SPECIFICATIONS:

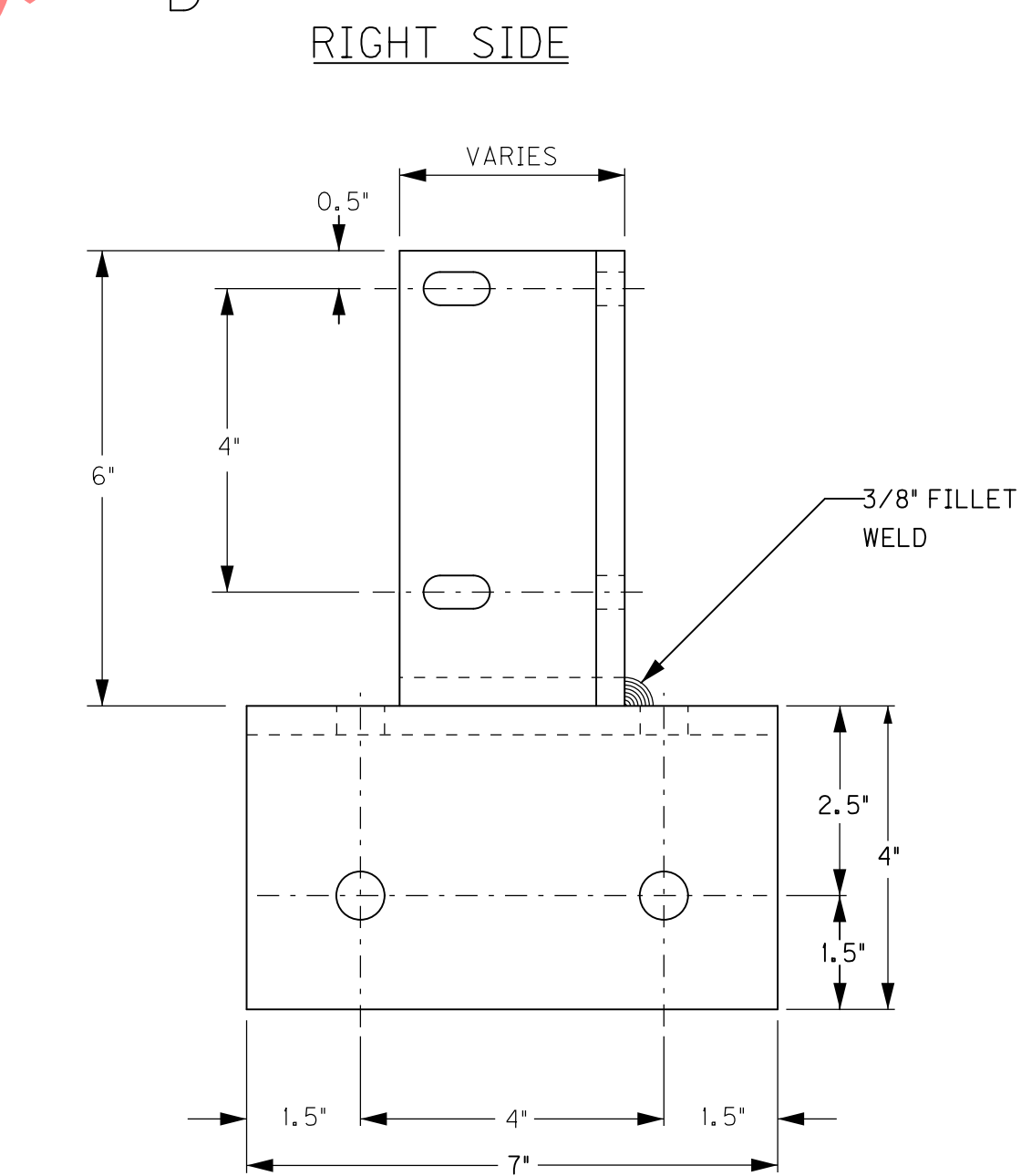
ALL STEEL TO BE FROM ASTM A572. ALL HOLES SHALL BE DRILLED. PLATE CUTS SHALL BE SAW CUT AND ALL EDGES GROUND SMOOTH. BOLT MATERIALS SHALL BE ASTM A307 OR EQUIVALENT AND BE GALVANIZED. GALVANIZE AFTER FABRICATION PER ASTM A123.



BARRIER WALL POST



ISOMETRIC VIEW



FRONT

NOTES:

ALL ROUND HOLES 5/8" Ø AND ALL OBLONG HOLES ARE 7/16" X 7/8".

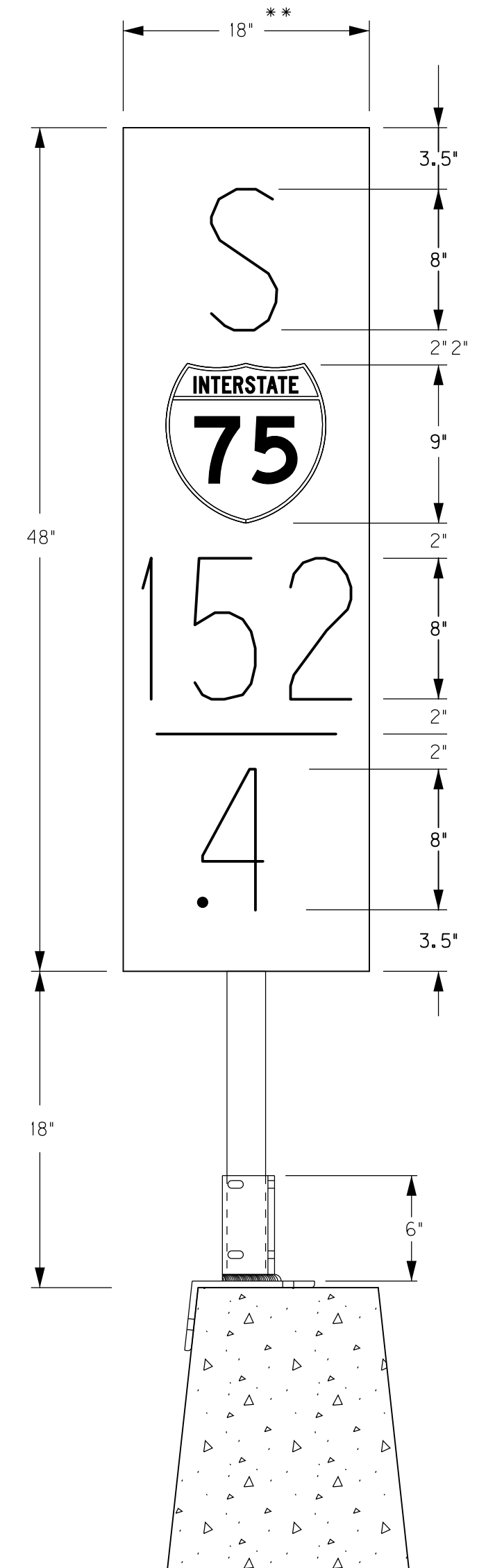
ALL HARDWARE SHALL BE GALVANIZED STEEL
OR STAINLESS STEEL AS SPECIFIED.

ALL WORK AND MATERIALS SHALL BE IN
ACCORDANCE WITH STANDARD SPECIFICATIONS.

ATTACH TO BARRIER WALL USING 1/2" X 13 TPI
GRADE 8 BOLTS WITH CONCRETE ANCHOR.

MATERIALS:

ASTM A36 STRUCTURAL ANGLE, 3/8" THICK WITH
3/8" FILLET WELD.



BARRIER MOUNTING BRACKET WITH REFERENCE MARKER

** 14" FOR 1 OR 2 DIGITS IN MILEPOINT.
18" FOR 3 DIGITS IN MILEPOINT.

BARRIER WALL MOUNTING DETAILS

NOT TO SCALE

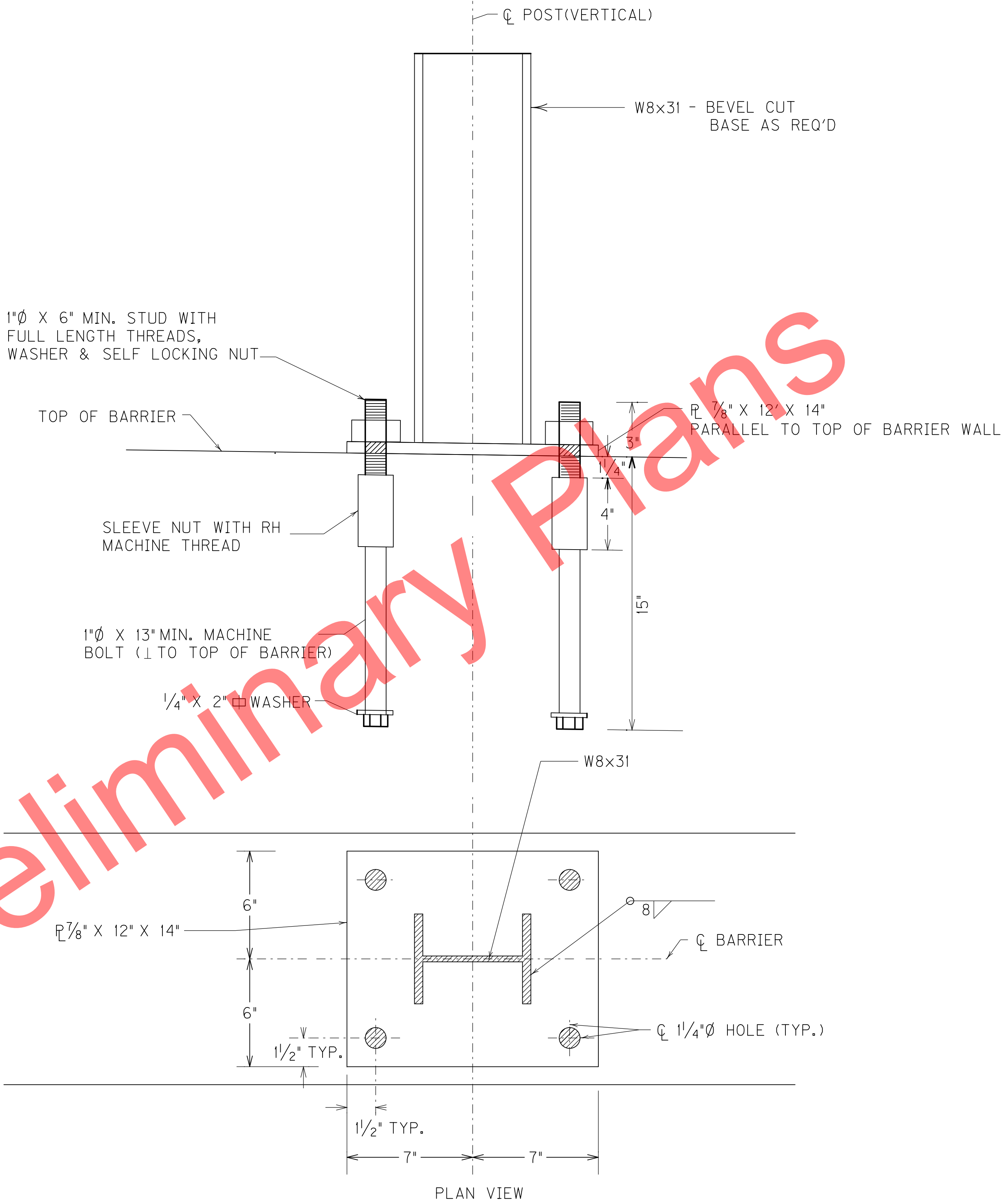
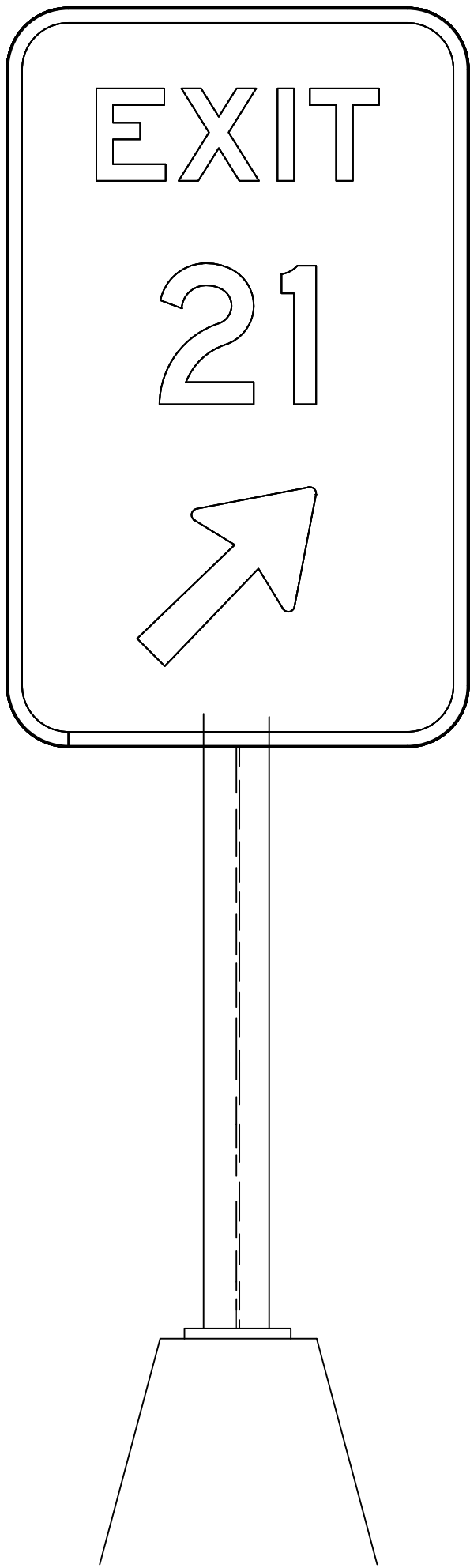
FILE NAME: P:\CIVIL\1471\SIGNS\SIGNING PLANS\T01200SN.DGN

USER: bteague
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8.11.7.443

THE COST FOR MAKING WALL POST AND ANY
HARDWARE REQUIRED TO COMPLETE THE
INSTALLATION SHALL BE INCLUDED IN THE
COST OF BARRIER WALL POST.



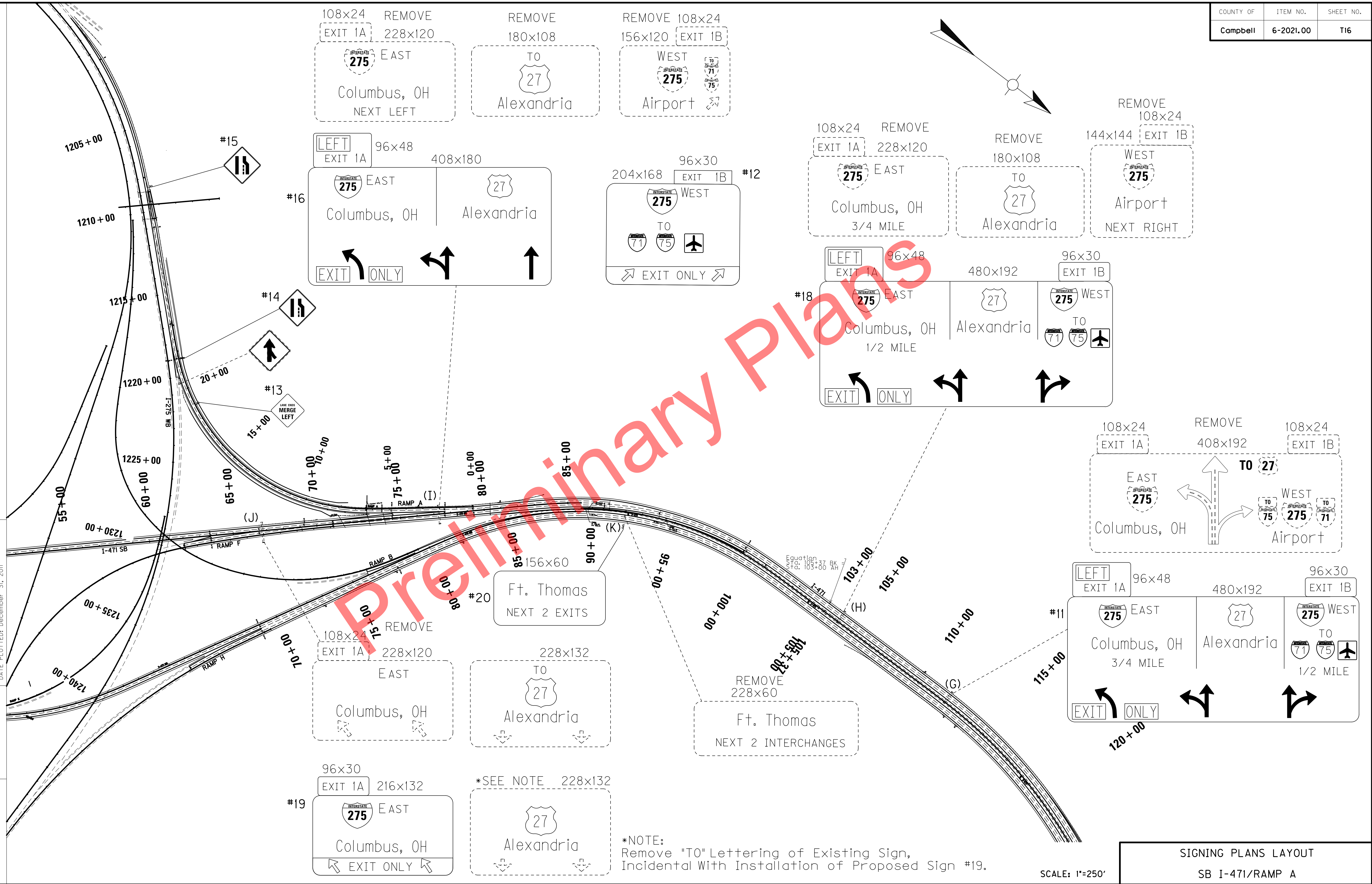
ANCHOR ALTERNATIVES
DRILL 1³/₈"Ø X 14" HOLE FOR
#29 X 16" REINFORCING BAR
WITH 1" X 4" MACHINE THREADS.
BOND ANCHORS IN HOLE WITH
POLYESTER RESIN.

MATERIAL SPECIFICATIONS
ALL STEEL TO BE FROM ASTM A572.
GALVANIZE AFTER FABRICATION PER
ASTM A123. ALL HOLES SHALL BE
DRILLED. PLATE CUTS SHALL BE
SAW CUT AND ALL EDGES GROUND
SMOOTH. BOLT MATERIALS SHALL
BE ASTM A307 OR EQUIVALENT AND
BE GALVANIZED.

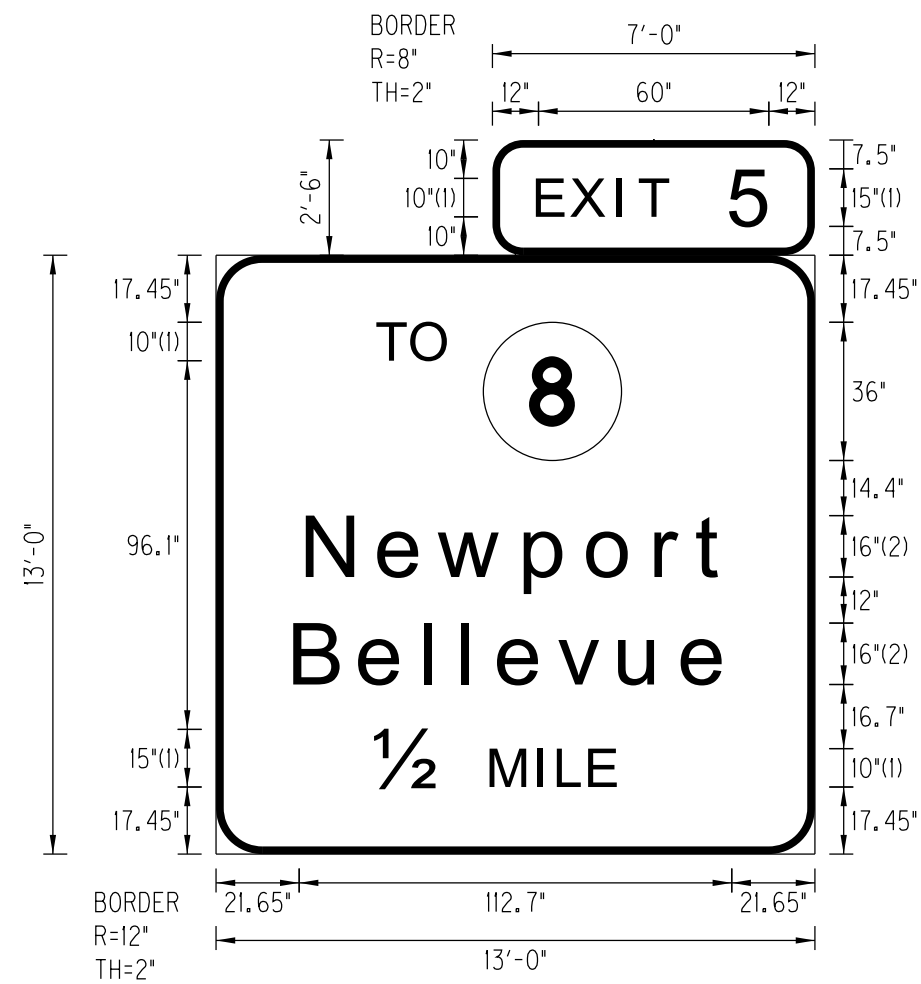
BEAM ATTACHMENT
SEE SIGNING MISCELLANEOUS
DETAIL SHEET FOR THE POST
CLIP ARRANGEMENT.

BARRIER WALL POST

FILE NAME: P:\CIVIL\1471\SIGNING PLANS\TOIG005N.DGN
USER: tvonbehren
DATE PLOTTED: December 31, 2011
E-SHEET NAME:
MicroStation v8.11.7.443



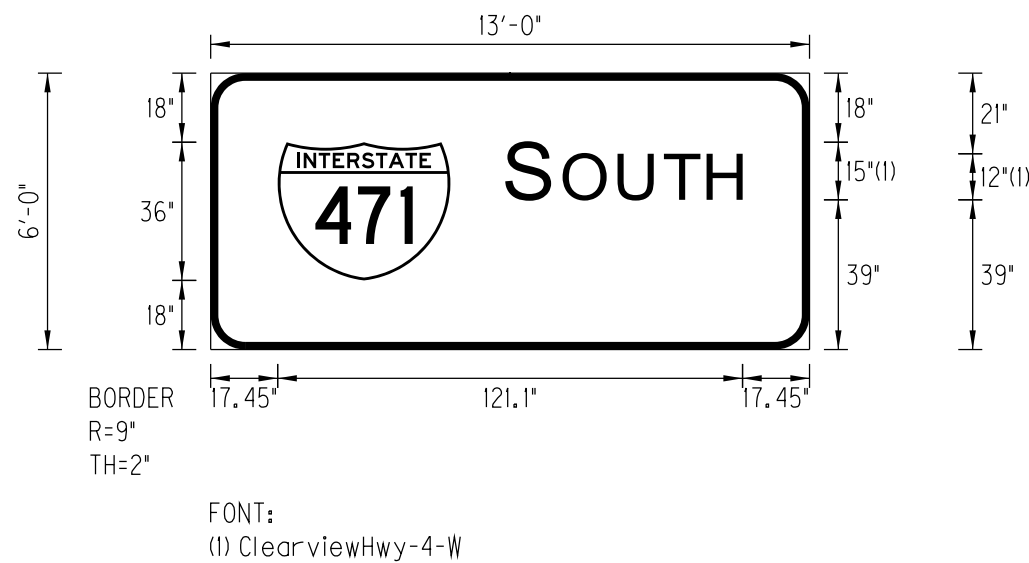
GENERAL NOTES:



FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W

#1

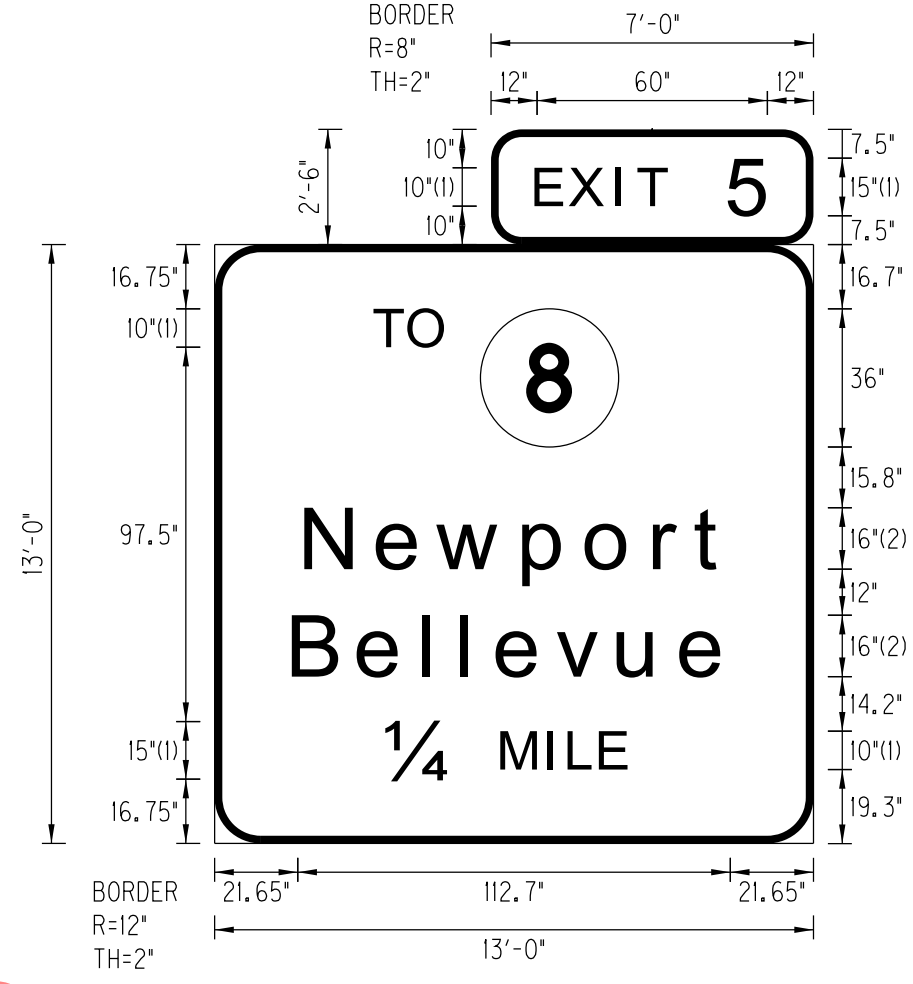
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	1	ROAD & MILE POINT	I-471 - 387' in Ohio	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	Overhead		
HEIGHT	13'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	186.5 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	36
STATIONS(S)	317+72	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			



FONT:
(1) ClearviewHwy-4-W

#2

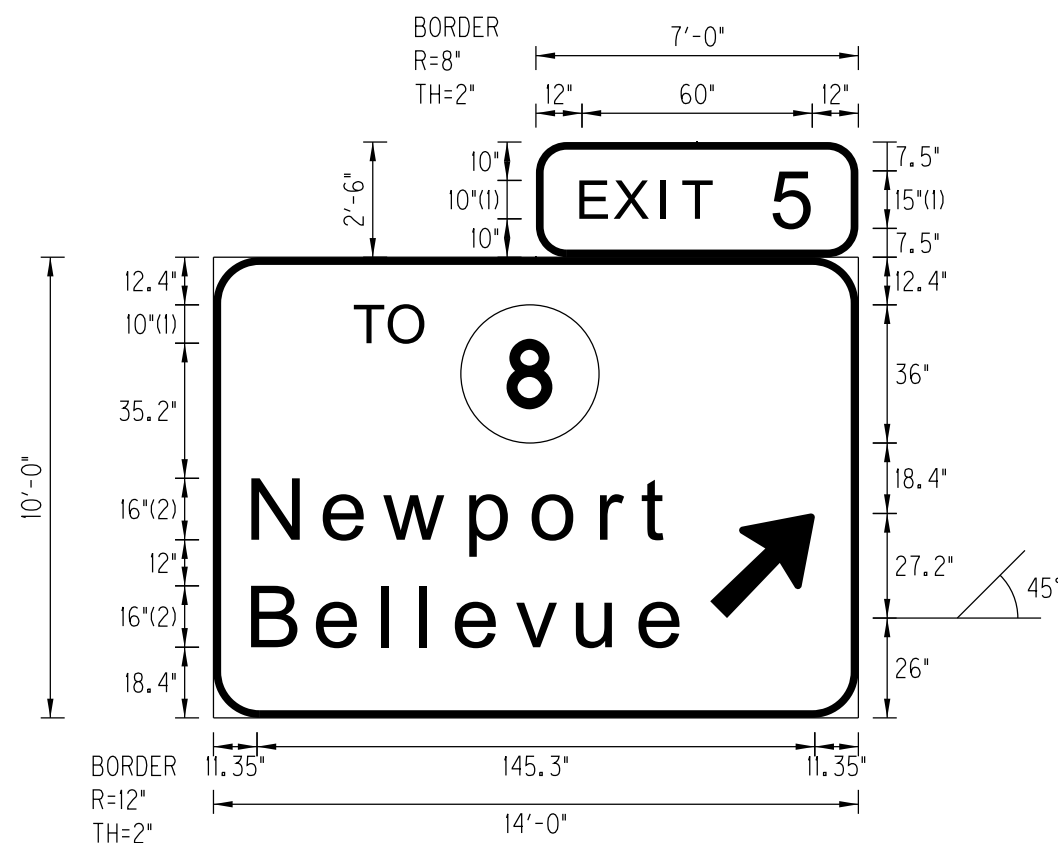
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	2	ROAD & MILE POINT	I-471 - 387' in Ohio	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	Overhead		
HEIGHT	6'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.)	78.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	9"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	45
STATIONS(S)	317+72	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			



FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W

#3

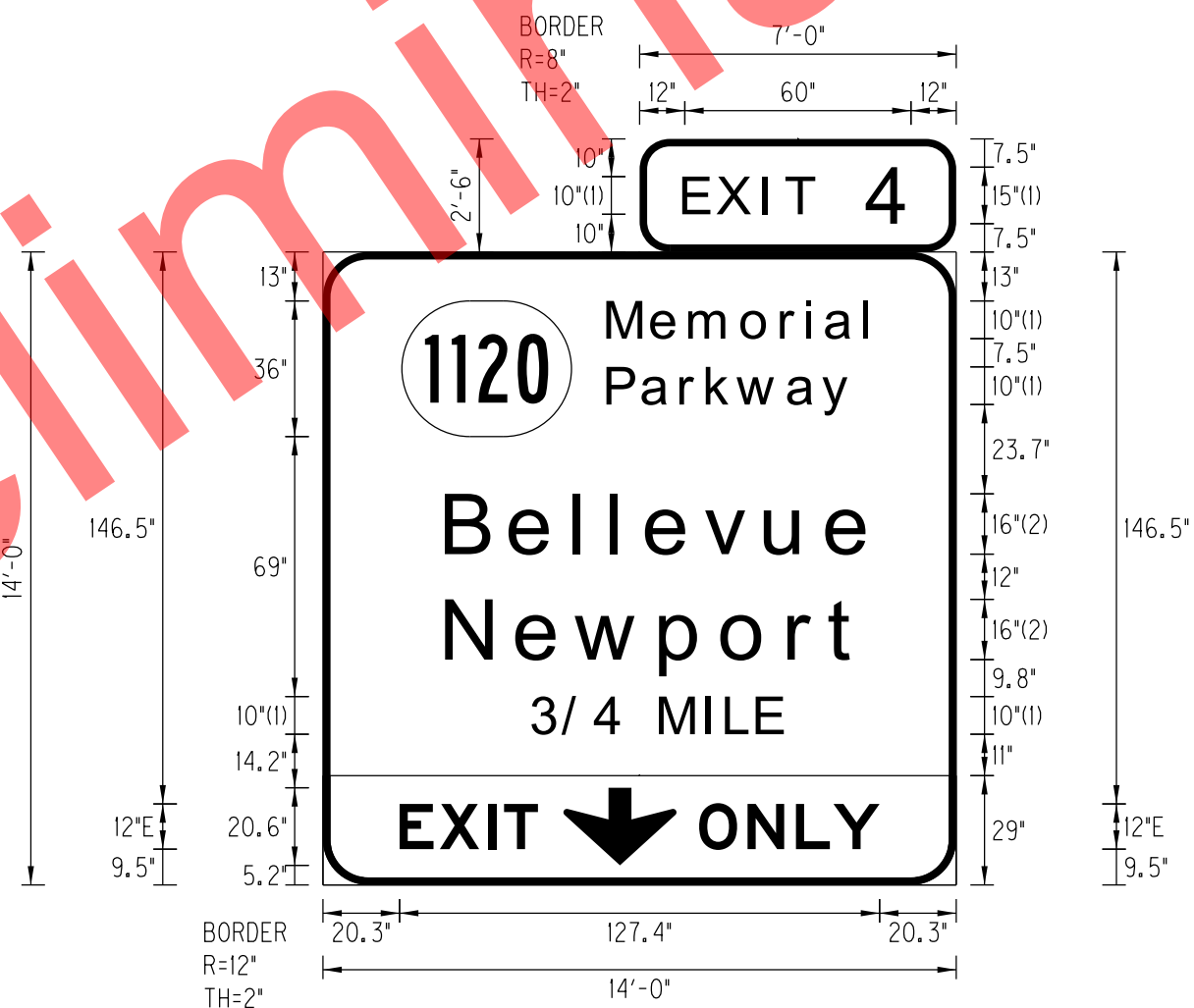
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	3	ROAD & MILE POINT	I-471 - mp 4.99	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	Overhead		
HEIGHT	13'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	186.5 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	36
STATIONS(S)	312+48	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			



FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W

#4

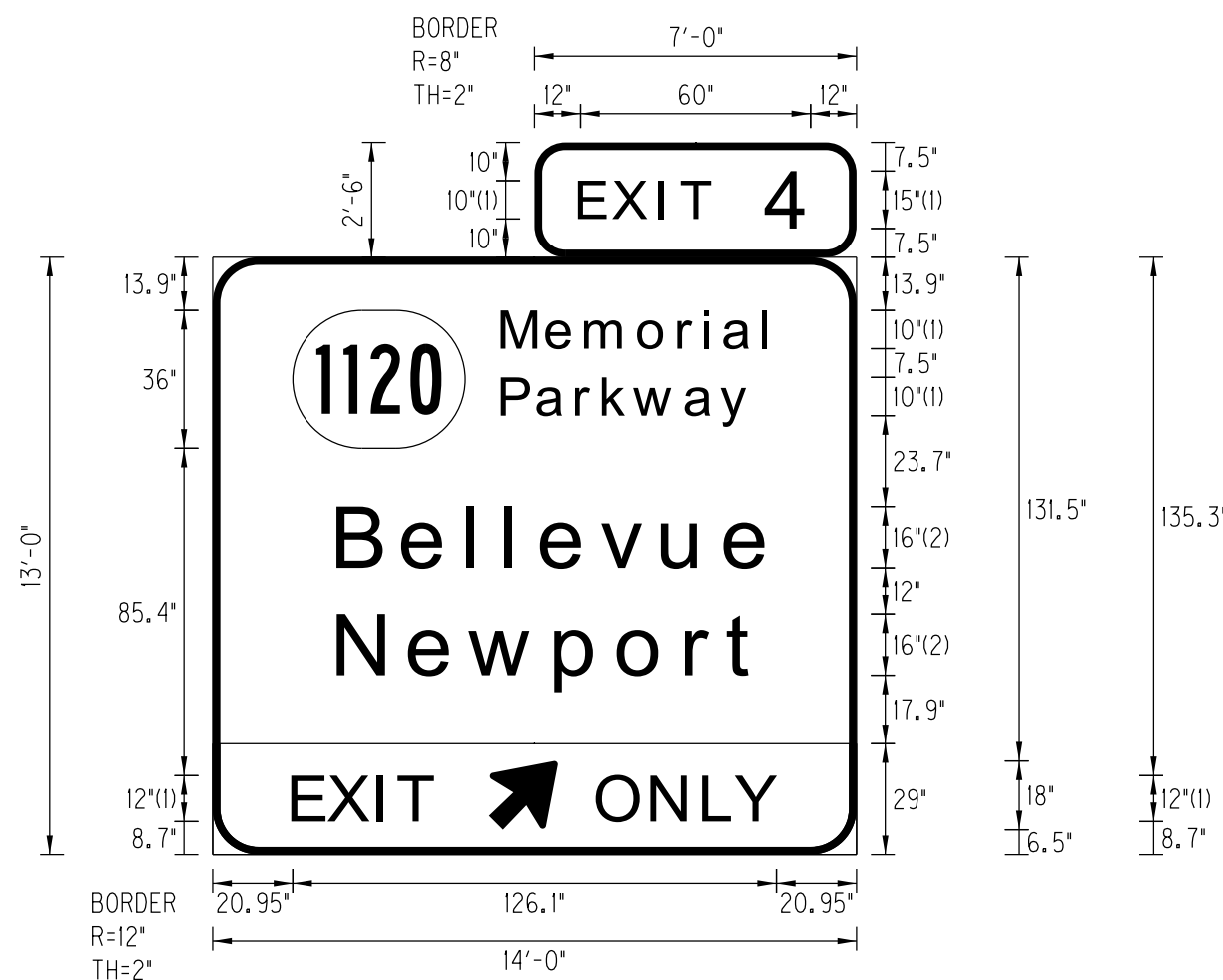
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	4	ROAD & MILE POINT	I-471 - mp 4.75	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	14'-0"	SIDE OF ROAD	Overhead		
HEIGHT	10'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	157.5 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	36
STATIONS(S)	299+64	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			



FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W

#17

SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	10	ROAD & MILE POINT	I-471 - mp 4.75	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	14'-0"	SIDE OF ROAD	Overhead		
HEIGHT	14'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	213.5 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	45
STATIONS(S)	299+64	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			



FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W

#10

SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	10	ROAD & MILE POINT	I-471 - mp 4.14	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	14'-0"	SIDE OF ROAD	Overhead		
HEIGHT	13'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	199.5 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	' LEFT
BORDER WIDTH	2"	BEAM SIZE			' RIGHT
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	SYMBOL	X
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		Y
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	WIDTH	45
STATIONS(S)	268+15	BEAM/POST LENGTH	4. =	HEIGHT	36
PANEL MATERIAL	Reflective	CONC 'a' =	Cu. Yds.		
LEGEND MATERIAL	Reflective	CONC 'b' =			

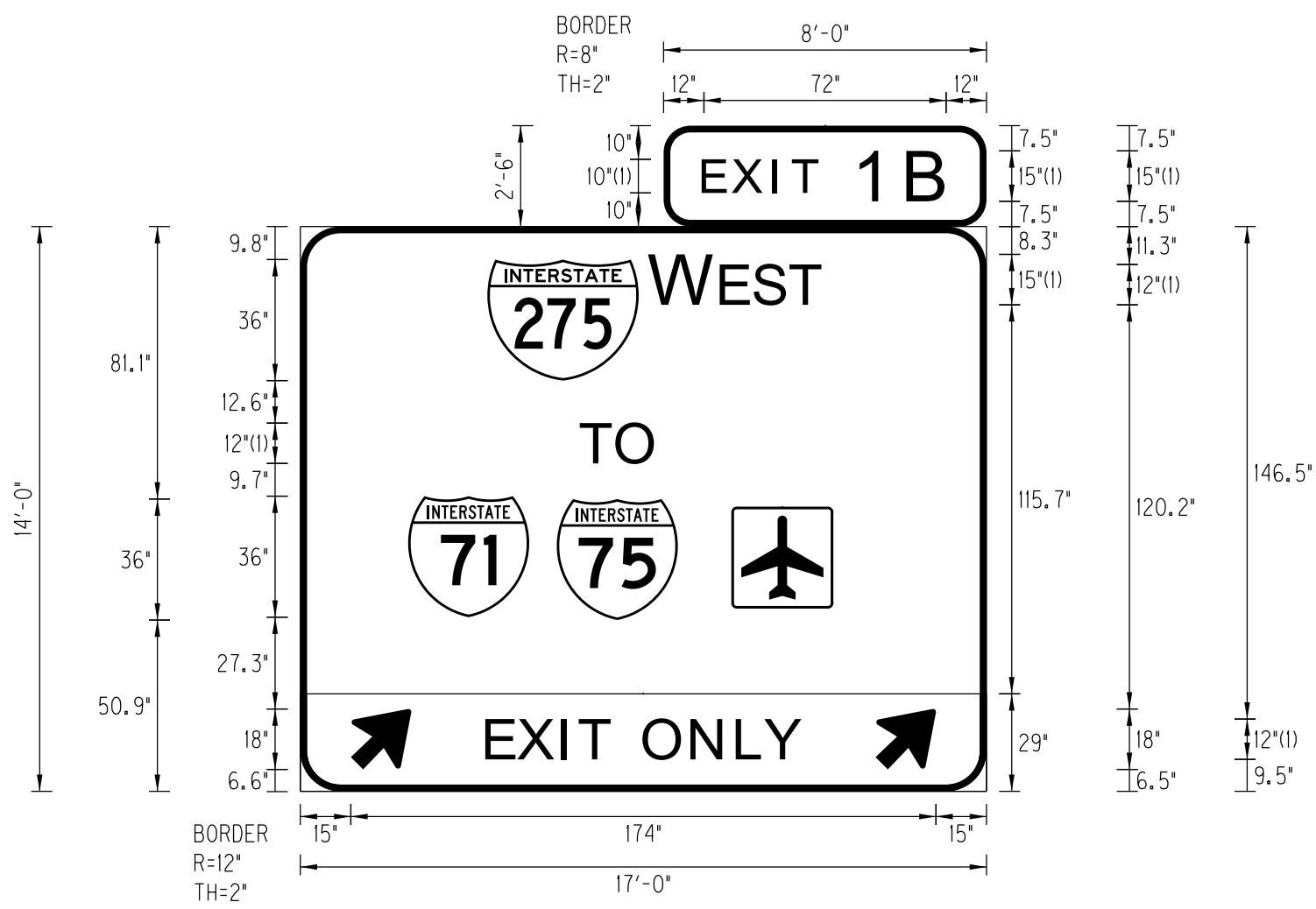
SIGN DETAIL SHT.

FILE NAME: P:\CIVIL\147\SIGNS\SIGNING PLANS\T018005N.DGN

USER: b'raque
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

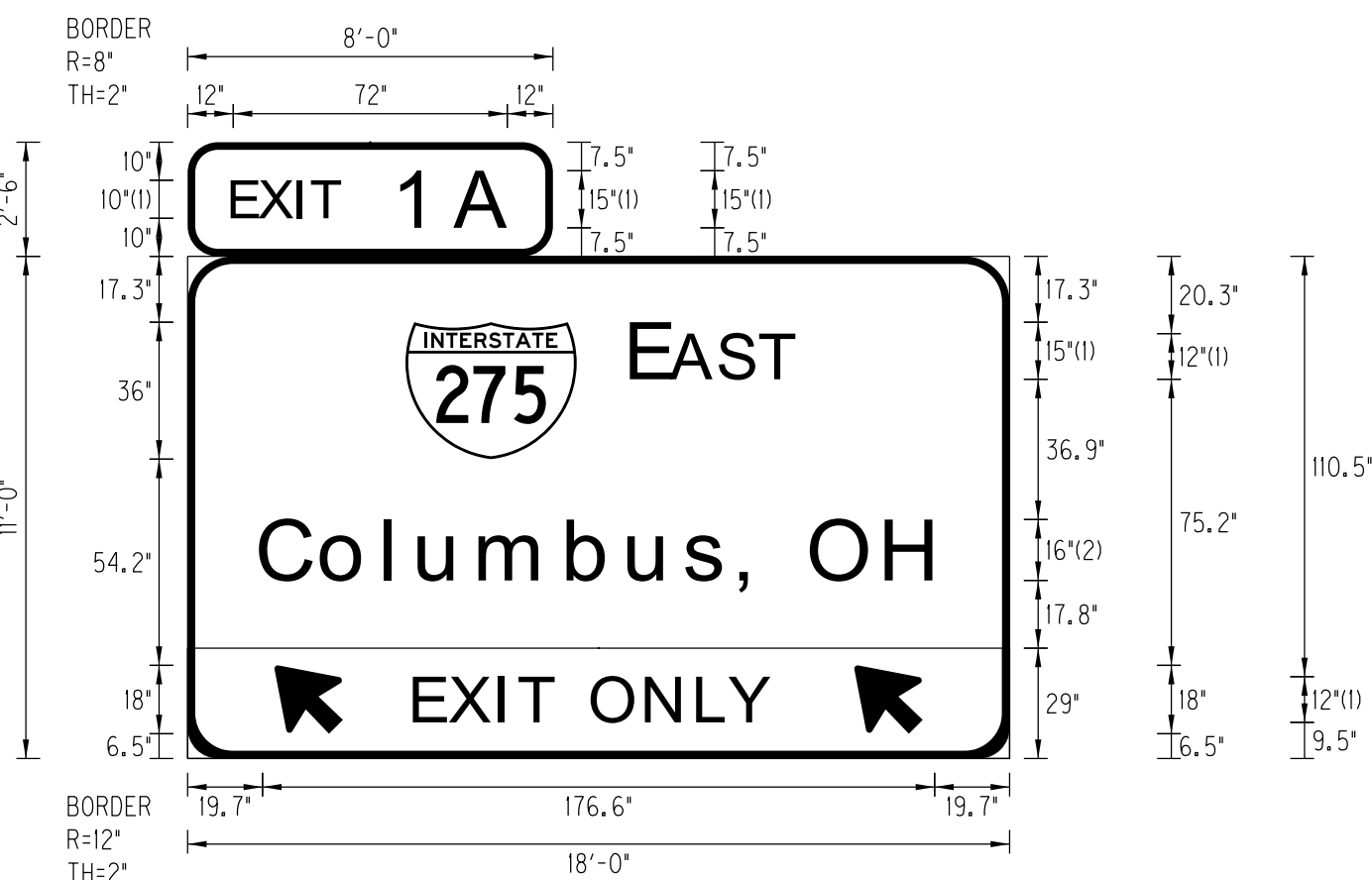
MicroStation v8.11.7.443



#12

FONT:
(1) ClearviewHwy-4-W

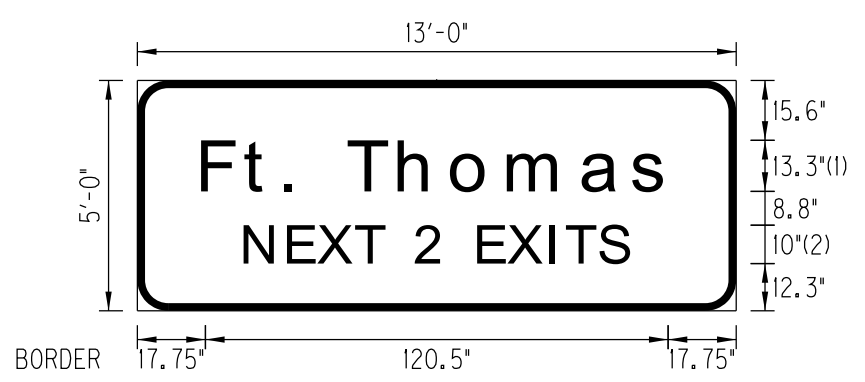
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	12	ROAD & MILE POINT	I-471 - mp 0.53	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	17'-0"	SIDE OF ROAD	Overhead		
HEIGHT	14'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.) -Total	258.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE			
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	77+31	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' =		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' =			



#19

FONT:
(1) ClearviewHwy-4-W
(2) ClearviewHwy-5-W-R

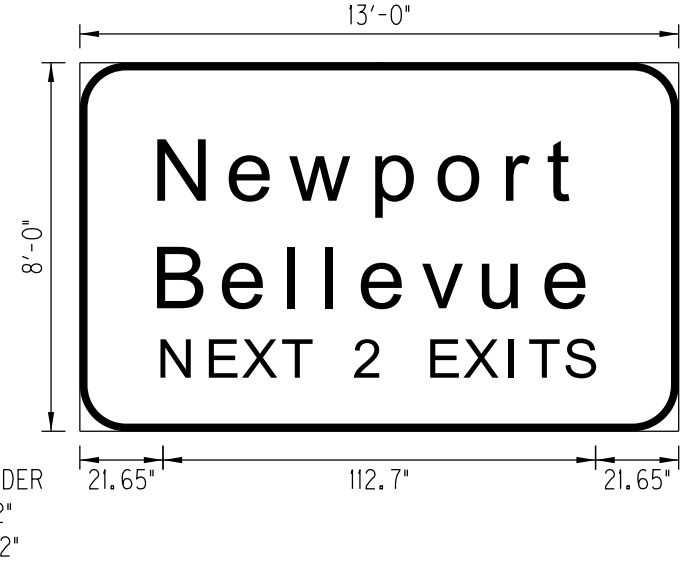
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	19	ROAD & MILE POINT	I-471 - mp 0.34	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	18'-0"	SIDE OF ROAD	Overhead		
HEIGHT	11'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.)	218.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE			
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	66+75	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' =		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' =			



#20

FONT:
(1) ClearviewHwy-5-W
(2) ClearviewHwy-4-W

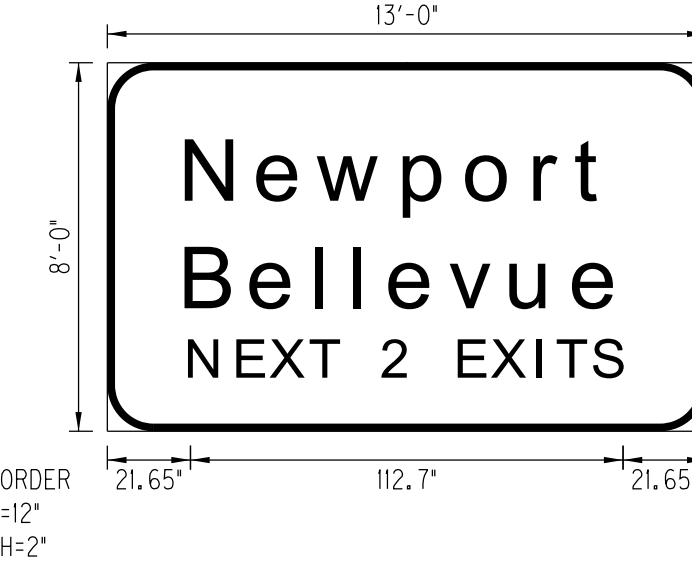
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	20	ROAD & MILE POINT	I-471 - mp 0.74	SUPPORT TYPE	B
QUANTITY	1	TRAFFIC DIRECTION	NB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	RIGHT		
HEIGHT	5'-0"	MOUNTING STYLE	GROUND		
AREA (Sq. Ft.)	65.0 SQ. FT.	BEAM MATERIAL	A36 Galvanized Steel	HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE	W8X18		
BORDER RADIUS	8"	BEAM/POST LENGTH	1. = 20.06 FT	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. = 12.00 FT		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	92+00	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' = 2'-6"		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' = 7'-0"			



#21

FONT:
(1) ClearviewHwy-5-W
(2) ClearviewHwy-4-W

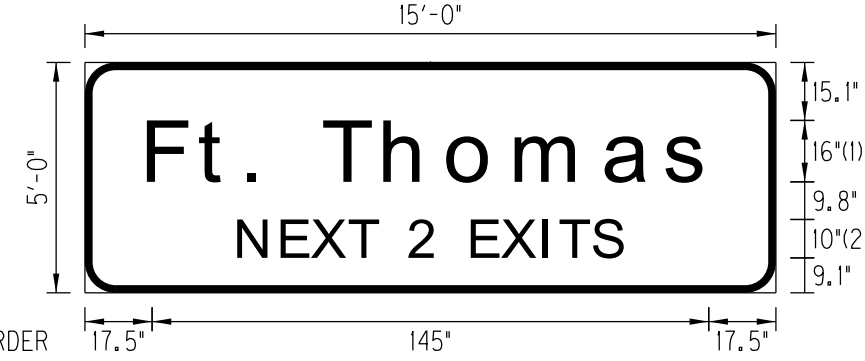
SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	21	ROAD & MILE POINT	I-471 - 387' in Ohio	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	Overhead		
HEIGHT	8'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.)	104.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE			
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	317+72	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' =		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' =			



#22

FONT:
(1) ClearviewHwy-5-W
(2) ClearviewHwy-4-W

SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	22	ROAD & MILE POINT	I-471 - mp 4.99	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	13'-0"	SIDE OF ROAD	Overhead		
HEIGHT	8'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.)	104.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE			
BORDER RADIUS	12"	BEAM/POST LENGTH	1. =	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	312+48	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' =		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' =			



#23

FONT:
(1) ClearviewHwy-5-W
(2) ClearviewHwy-4-W

SIGN INFORMATION		SIGN LOCATION / SUPPORT		SUPPORT TYPE	
SIGN NUMBER	23	ROAD & MILE POINT	I-471 - mp 4.14	SUPPORT TYPE	Existing Truss
QUANTITY	1	TRAFFIC DIRECTION	SB	*A* = FIXED BEAMS *B* = STANDARD BREAK-A-WAY BEAMS *C* = OMNI-DIRECTIONAL BREAK-A-WAY	
WIDTH	15'-0"	SIDE OF ROAD	Overhead		
HEIGHT	5'-0"	MOUNTING STYLE	Existing Truss		
AREA (Sq. Ft.)	75.0 SQ. FT.	BEAM MATERIAL		HORIZONTAL CLEARANCE	
BORDER WIDTH	2'	BEAM SIZE			
BORDER RADIUS	8"	BEAM/POST LENGTH	1. =	' LEFT	
PANEL COLOR	Green	BEAM/POST LENGTH	2. =		
LEGEND/BORDER COLOR	White/White	BEAM/POST LENGTH	3. =	' RIGHT	
STATIONS(S)	268+15	BEAM/POST LENGTH	4. =		
PANEL MATERIAL	Reflective	CONC 'a' =		SYMBOL	
LEGEND MATERIAL	Reflective	CONC 'b' =			

GENERAL NOTES:

SIGN DETAIL SHT.

FILE NAME: P:\CIVIL\1471\SIGNS\SIGNING PLANS\T019005N.DGN

USER: tvonbehren
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

MicroStation v8.11.7.443

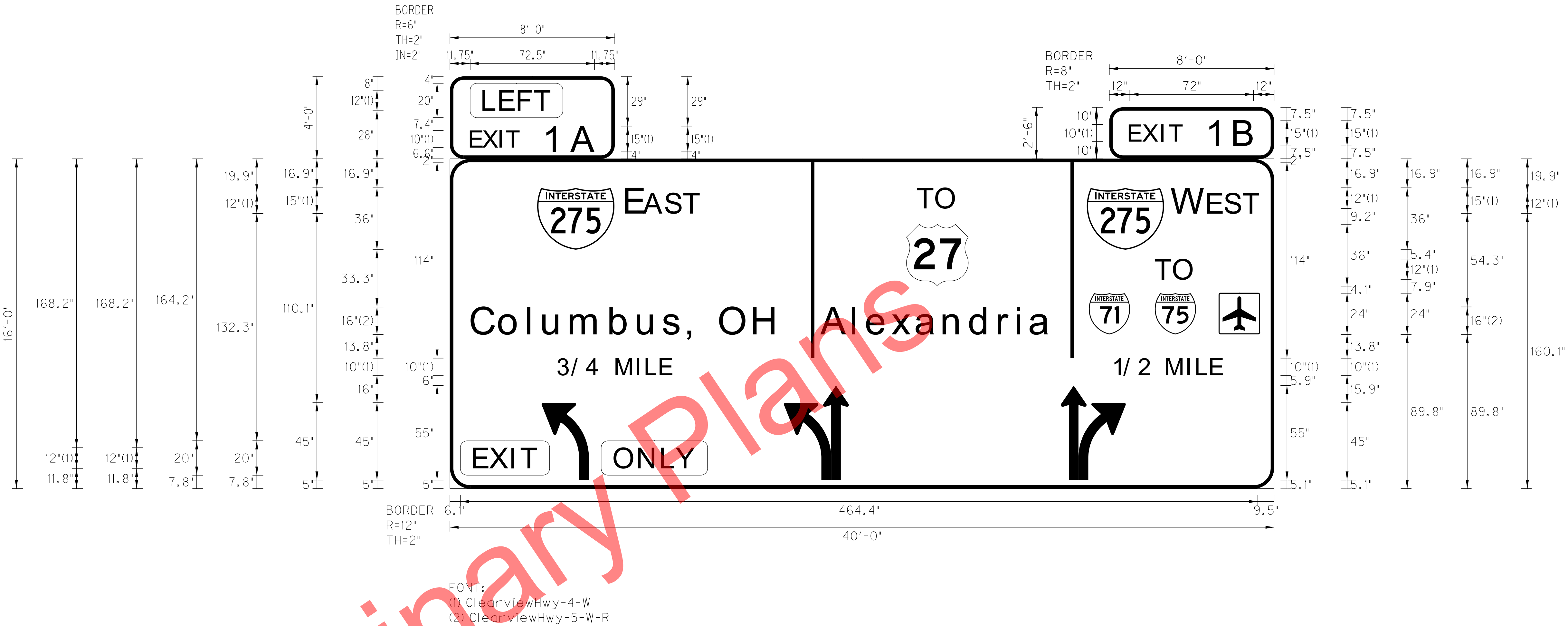
GENERAL SIGN INFORMATION	
SIGN NUMBER(S)	11
QUANTITY	1
WIDTH	40'-0"
HEIGHT	16'-0"
AREA (Sq. Ft.)Total	692.0 Sq. Ft.
BORDER WIDTH	2"
BORDER RADII	12"
PANEL COLOR	Green
LEGEND/BORDER COLOR	White/White
STATIONS(S)	112+00 & 104+00
PANEL MATERIAL	Reflective
LEGEND MATERIAL	Reflective
PANEL STYLE	guide_fwy_overhead.ssi

SYMBOL (S)	X	Y	WIDTH	HEIGHT
MI_1	50.8	139.1	45	36
MI_1	370.9	139.1	45	36
MI_4	265.9	117.9	36	36
MI_1	372.1	89.9	24	24
MI_1	410.5	89.9	24	24
I-5			24	24
ARPCRI	53.4	5	27.5	45
ARPCRI	194.5	5	27.5	45
ARUP	217.3	5	15	55
ARUP	355.8	5	15	55
ARPCRI	366.1	5	27.5	45

LETTER SPACING/INFORMATION

S= 12	COPY	L	E	F	T																		
L= 37.8	SPACE	19.9	29.5	40.3	49.6																		
S= 10/15	COPY	E	X	I	T	I	A																
L= 72	SPACE	12.3	19.6	29.5	33.2	54.9	71.2																
S= 10/15	COPY	E	X	I	T	I	B																
L= 72	SPACE	12	20.1	30.8	35.4	57.1	73.3																
S= 12/15	COPY	E	A	S	T													W	E	S	T		
L= 41.6,49.1	SPACE	103	113.3	125.9	136.6													417.9	438.4	448.3	459		
S= 12	COPY	T	O																	T	O		
L= 21.2,21.2	SPACE	272.7	283.5																	407.7	418.5		
S= 16	COPY	C	o	l	u	m	b	u	s	,		O	H										
L= 176.6	SPACE	13	29.2	46	54.8	70.8	94	110	124.5	137.9	142.1	157.8	177.4										
S= 16	COPY	A	I	e	x	a	n	d	r	i	a												
L= 133.5	SPACE	216.7	235.2	243.5	257.6	272	288	303.5	320.2	330.8	338.4												
S= 10	COPY	3	/	4	M	I	L	E									1	/	2	M	I	L	E
L= 65.8, 63.2	SPACE	62.8	71.5	79.8	97.2	109.3	114.7	122.8									386.2	393.1	401.5	417.9	430	435.4	443.5
S= 12	COPY	E	X	I	T														O	N	L	Y	
L= 35.6,45.8	SPACE	14.3	23.8	36.5	41.9														96.1	110.4	123.9	132.3	

11



SIGN LOCATION & MOUNTING INFORMATION			
SIDE OF ROAD	TRAFFIC DIRECTION	ON ROAD	MILE POINT
Overhead	SB	I-471	1.17
MOUNTING STYLE	Existing Truss		
BEAM MATERIAL			
BEAM SIZE			
BEAM/POST 1 LENGTH =		HORIZONTAL CLEARANCE	
BEAM/POST 2 LENGTH =			
BEAM/POST 3 LENGTH =		' LT.	' RT.
TYPE "A" FIXED		TYPE "B" BREAK-A-WAY	
CONCRETE BASE DIMENSIONS			
a = DIA.	b =	Cu. Yds.	

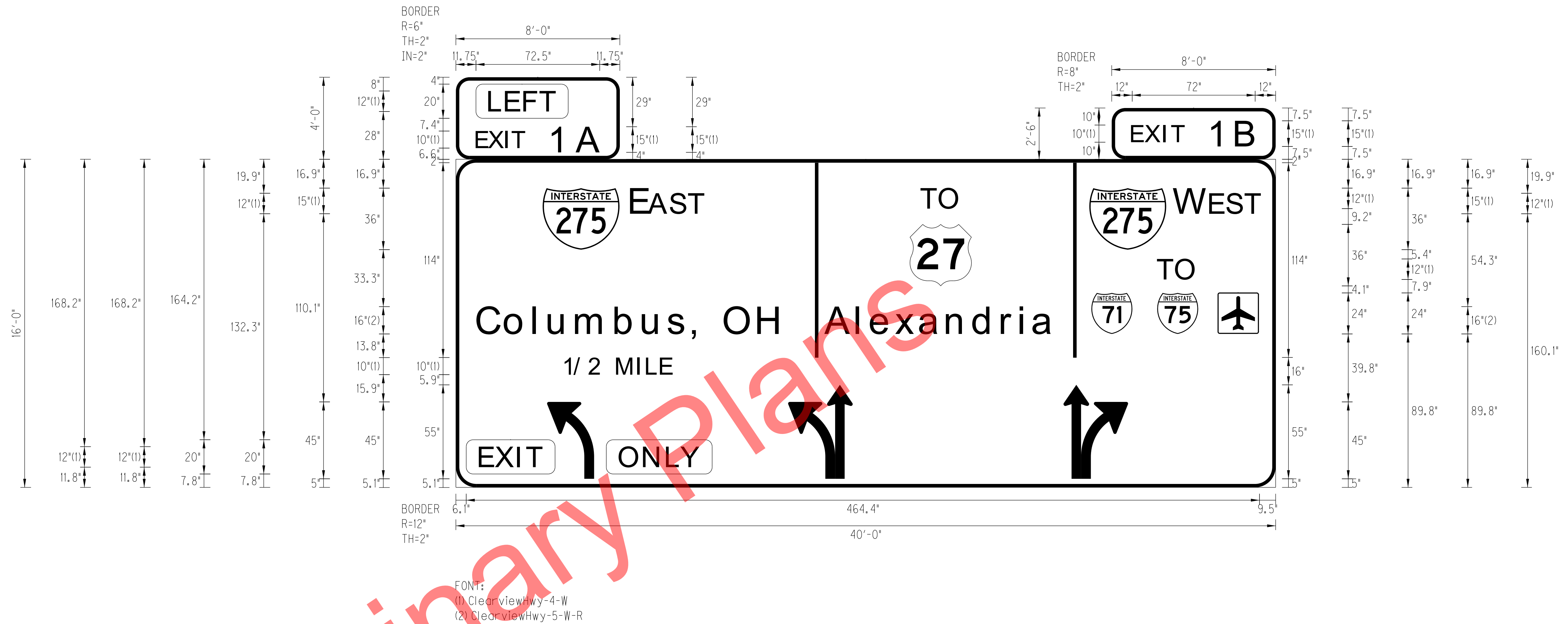
COUNTY OF	ITEM NO.	SHEET NO.
Campbell	6-2021.00	T20

GENERAL SIGN INFORMATION	
SIGN NUMBER(S)	18
QUANTITY	1
WIDTH	40' - 0"
HEIGHT	16' - 0"
AREA (Sq. Ft.) Total	692.0 Sq. Ft.
BORDER WIDTH	2"
BORDER RADII	12"
PANEL COLOR	Green
LEGEND/BORDER COLOR	White/White
STATIONS(S)	112+00 & 104+00
PANEL MATERIAL	Reflective
LEGEND MATERIAL	Reflective
PANEL STYLE	guide_fwy_overhead.ssi

SYMBOL (S)	X	Y	WIDTH	HEIGHT
MI_1	50.8	139.1	45	36
MI_1	370.9	139.1	45	36
MI_4	265.9	117.9	36	36
MI_1	372.1	89.8	24	24
MI_1	410.6	89.8	24	24
1-5			24	24
ARPCRI	53.4	5	27.5	45
ARPCRI	194.5	5	27.5	45
ARUP	217.3	5	15	55
ARUP	355.8	5	15	55
ARPCRI	366.1	5	27.5	45

LETTER SPACING/INFORMATION

S= 12	COPY	L	E	F	T																
L= 37.8	SPACE	19.9	29.5	40.3	49.6																
S= 10/15	COPY	E	X	I	T	l	A														
L= 72	SPACE	12.3	19.6	29.5	33.2	54.9	71.2														
S= 10/15	COPY	E	X	I	T	l	B														
L= 72	SPACE	12	20.1	30.8	35.4	57.1	73.3														
S= 12/15	COPY	E	A	S	T												W	E	S	T	
L= 41.6,49.1	SPACE	103	113.3	125.9	136.6												417.9	438.4	448.3	459	
S= 12	COPY	T	O																T	O	
L= 21.2,21.2	SPACE	272.7	283.5																407.7	418.5	
S= 16	COPY	C	o	l	u	m	b	u	s	,		O	H								
L= 176.6	SPACE	13	29.2	46	54.8	70.8	94	110	124.5	137.9	142.1	157.8	177.4								
S= 16	COPY	A	l	e	x	a	n	d	r	i	a										
L= 133.5	SPACE	216.7	235.2	243.5	257.6	272	288	303.5	320.2	330.8	338.4										
S= 10	COPY	l	/	2	M	I	L	E													
L= 63.2	SPACE	62.8	69.7	78.1	94.5	106.7	112.1	120.1													
S= 12	COPY	E	X	I	T													O	N	L	Y
L= 35.6,45.8	SPACE	14.3	23.8	36.5	41.9													96.1	110.4	123.9	132.3



SIGN LOCATION & MOUNTING INFORMATION			
SIDE OF ROAD	TRAFFIC DIRECTION	ON ROAD	MILE POINT
Overhead	SB	I-471	1.01
MOUNTING STYLE		Existing Truss	
BEAM MATERIAL			
BEAM SIZE			
BEAM/POST 1 LENGTH =		HORIZONTAL CLEARANCE	
BEAM/POST 2 LENGTH =			
BEAM/POST 3 LENGTH =			
		' LT.	' RT.
TYPE "A" FIXED		TYPE "B" BREAK-A-WAY	
CONCRETE BASE DIMENSIONS			
a = DIA.	b =	Cu. Yds.	

FILE NAME: P:\CIVIL\I47\SIGNS\SIGNING PLANS\T02000SN.DGN

USER: tvonbehren
DATE PLOTTED: December 31, 2011

E-SHEET NAME:

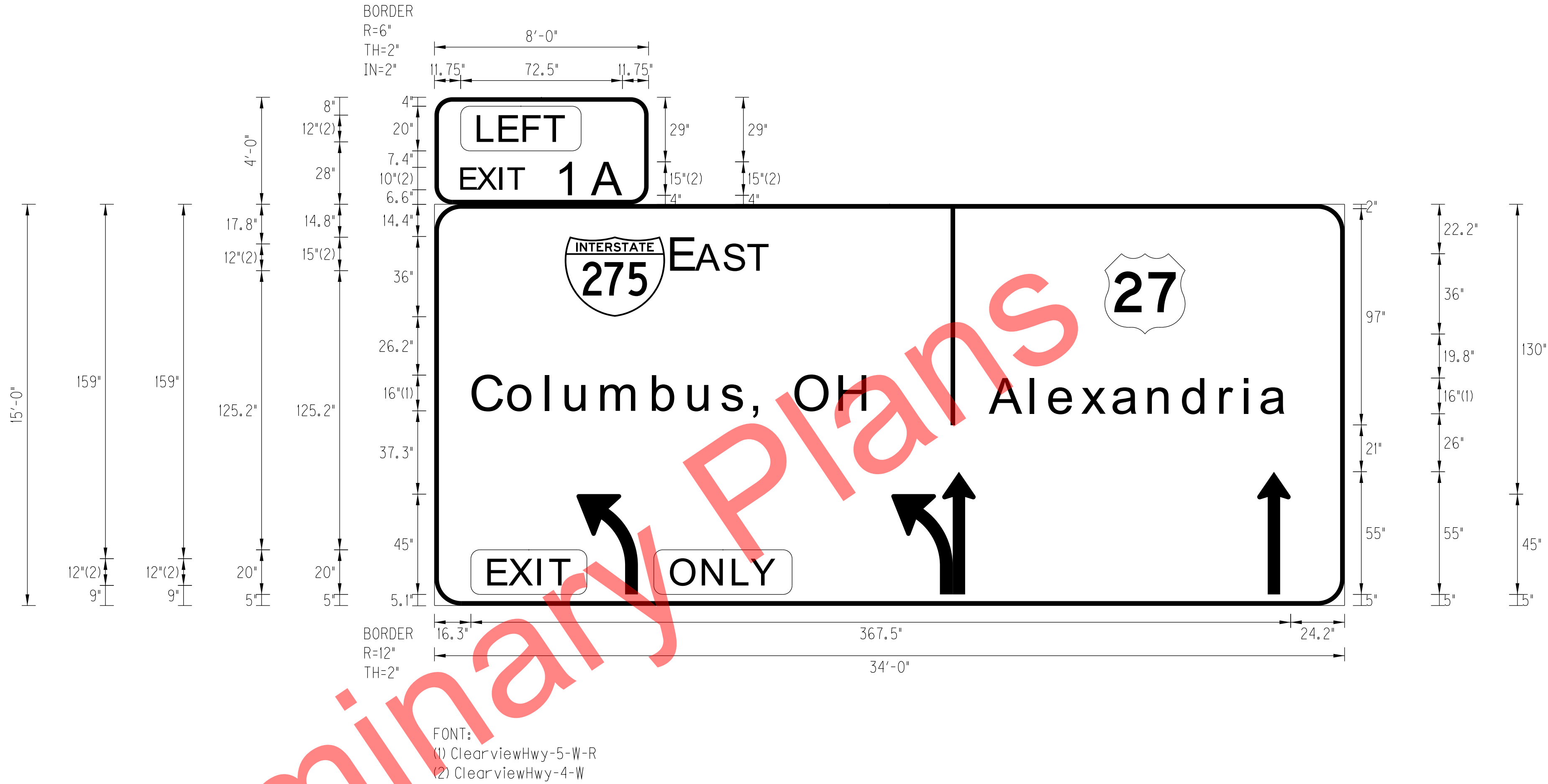
MicroStation v8.11.7.443

GENERAL SIGN INFORMATION	
SIGN NUMBER	16
QUANTITY	1
WIDTH	34'-0"
HEIGHT	15'-0"
AREA (Sq. Ft.)Total	542.0 Sq. Ft.
BORDER WIDTH	2"
BORDER RADII	12"
PANEL COLOR	Green
LEGEND/BORDER COLOR	White/White
STATIONS(S)	77+31
PANEL MATERIAL	Reflective
LEGEND MATERIAL	Reflective
PANEL STYLE	guide_fwy_overhead.ssi

SYMBOL (S)	X	Y	WIDTH	HEIGHT
MI_1	58.4	129.6	45	36
MI_4	300.5	121.8	36	36
ARPCR1	63.7	5	27.5	45
ARPCR1	204.8	5	27.5	45
ARUP	227.6	5	15	55
ARUP	368.8	5	15	55

LETTER SPACING/INFORMATION


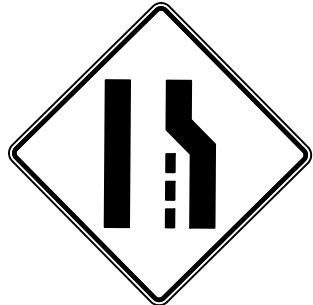


S= 12	COPY	L	E	F	T																
L= 37.8	SPACE	19.9	29.5	40.3	49.6																
S= 10/15	COPY	E	X	I	T	I	A														
L= 72	SPACE	12.2	19.6	29.5	33.2	54.9	71.2														
S= 12/15	COPY	E	A	S	T																
L= 42.3	SPACE	106.7	117.6	130.3	141																
S= 16	COPY	C	o	l	u	m	b	u	s	,		0	H								
L= 176.6	SPACE	17.6	33.8	50.5	59.4	75.4	98.5	114.5	129.1	142.2	146.7	162.4	182								
S= 16	COPY	A	l	e	x	a	n	d	r	i	a										
L= 133.5	SPACE	249.5	268	276.2	290.3	304.7	320.7	336.2	352.9	363.6	371.1										
S= 12	COPY	E	X	I	T													0	N	L	Y
L= 35.6,45.8	SPACE	24.5	34.1	46.7	52.1													106.4	120.7	134.2	142.6



SIGN LOCATION & MOUNTING INFORMATION			
SIDE OF ROAD	TRAFFIC DIRECTION	ON ROAD	MILE POINT
Overhead	SB	I-471	0.53
MOUNTING STYLE		Existing Truss	
BEAM MATERIAL			
BEAM SIZE			
BEAM/POST 1 LENGTH =		HORIZONTAL CLEARANCE	
BEAM/POST 2 LENGTH =			
BEAM/POST 3 LENGTH =		' LT.	' RT.
TYPE "A" FIXED		TYPE "B" BREAK-A-WAY	
CONCRETE BASE DIMENSIONS			
a =	DIA.	b =	Cu. Yds.

SHEETING SIGNS DETAIL SHEET

COUNTY OF	ITEM NO.	SHEET NO.
Campbell	6-2021.00	T22

SIGN/SIGN ASSEMBLY NUMBER	SIZES IN INCHES		MESSAGES ①	SPECIFICATION	SIGN LOCATION			
	HORIZ.	VERT.			SIDE OF ROAD	FACING TRAFFIC TRAVELING	ON ROAD	AT STATION (APPROXIMATE)
S----								
S----								
S-7				W4-5	RT	SB	I-471	287+94
				REMOVE Existing 48x48 sign (Removal Incidental to Project)				
S-8	48	48		W4-2	RT	SB	I-471	279+82
				(Attach to Barrier Wall)				
				(500' in front of Ramp Nose)				
S-9	36	36		R3-7	RT	SB	I-471	270+38
				(Attach to Barrier Wall)				
				(400' from last Full Taper)				
S-13	48	48		W9-2	RT	SB	Ramp A	18+89
				(1100' back from last 5-lane				
				Full Taper on I-275)				
				2 POSTS				

SIGN/SIGN ASSEMBLY NUMBER	SIZES IN INCHES		MESSAGES ①	SPECIFICATION	SIGN LOCATION			
	HORIZ.	VERT.			SIDE OF ROAD	FACING TRAFFIC TRAVELING	ON ROAD	AT STATION (APPROXIMATE)
S-14	48	48		W4-2	RT	SB	Ramp A	21+80
				(800' back from last 5-lane				
				Full Taper on I-275)				
				2 POSTS				
S-15	48	48		W4-2	RT	SB	I-275	1208+58
				(800' back from last 4-lane				
				Full Taper on I-275)				
				2 POSTS				
S-								
S-								
S-								

SPECIAL NOTES:

1. THE COLORS SHALL BE AS SHOWN IN THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" AND "STANDARD HIGHWAY SIGNS".
2. SHEETING SIGNS SHALL BE FABRICATED FROM 0.125 GAUGE MATERIAL IF ANY DIMENSION IS GREATER THAN 36" (THIRTY-SIX INCHES), OTHERWISE 0.080 GAUGE MATERIAL SHALL BE USED.
3. SHEETING SIGNS SHALL BE MOUNTED ON TYPE II POST USING STANDARD INSTALLATION IN SOIL, WITH SOIL STABILIZER. SEE THE SHEETING SIGN DETAIL SHEETS.